

**TAMPA-HILLSBOROUGH COUNTY  
EXPRESSWAY AUTHORITY**

**Addendum No. 10**

**FOR**

**Request for Proposals (RFP)**

**East Selmon Slip Ramps Design-Build**

**RFP O-02520**

**ADDENDUM NO. 10**

PROJECT NAME: East Selmon Slip Ramps Design-Build RFP O-02520

DATE OF ADDENDUM: March 1, 2021

**PLEASE NOTE THE FOLLOWING ADDENDUM NO. 10 TO THE RFP:**

**ATTACHMENTS**

The Attachments listed below are hereby incorporated into and made a part of this Request for Proposal (RFP) as though fully set forth herein.

A\_001 - Project Advertisement

A\_002 - Division I Design-Build Specifications

Division I Special Provisions identified by the Authority for this Project:

A\_003.01 - Award and Execution of Contract (SP0030200)

A\_003.02 - Public Records (SP0030900)

A\_003.03 - Permits and Licenses (No free passes will be issued to the Contractor for use on the Toll Facility)  
(SP0070201)

A\_003.04 - Preservation of Property for Toll Facilities (SP0071101-tolls)

A\_003.05 - Equal Employment Opportunity Requirements (SP0072700)

A\_003.06 - Preference to State Residents (SP0072800)

A\_003.07 - Legal Requirements and Responsibility to the Public - E-Verify (SP0072900)

A\_003.08 - Legal Requirements and Responsibility to the Public - Scrutinized Companies (SP0073000)

A\_003.09 - Contaminated Material – Mercury-Containing Devices and Lamps (SP0080409)

A\_003.10 - Prosecution and Progress - Damage Recovery (SP0081200)

FDOT Divisions II and III Special Provisions identified by the Authority for this Project:

A\_004.01 - Mobilization (SP1010000DB)

A\_004.02 - Contractor Quality Control General Requirements (SP1050813DB)

A\_004.03 - Structures Foundations (SP4550000DB)

A\_004.04 – Value Added Bridge Components (Dev475)

A\_005 - City of Tampa Truck Routes

A\_006 – THEA General Tolling Requirements

A\_007 – Letters of Clarification (pending)

THEA Forms

A\_008.01 - Bid Blank, Design Build Major

A\_008.02 - Dispute Review Board Three Party Agreement

A\_008.03 - Certificate of Insurance

A\_008.04 - Insurance Requirements, Coverages, and Limits

A\_008.05 - Anticipated SBE Participation Statement

A\_008.06 – Schedule of Values

Conceptual Typical Sections

A\_009.01 – NOT USED

A\_009.02 – Conceptual Slip Ramp Typical Sections (Updated March 1, 2021)

A\_010 – Pipe Lining Locations

A\_011 – Pre-Bid Meeting Presentation

**The RFP Reference Documents are revised as follows:**

## **REFERENCE DOCUMENTS**

The following documents are being provided with this RFP. Except as specifically set forth in the body of this RFP, these documents are being provided for reference and general information only. They are not being incorporated into and are not being made part of the RFP, the contract documents or any other document that is connected or related to this Project except as otherwise specifically stated herein. No information contained in these documents shall be construed as a representation of any field condition or any statement of facts upon which the Design-Build Firm can rely upon in performance of this contract. All information contained in these reference documents must be verified by a proper factual investigation. The bidder agrees that by accepting copies of the documents, any and all claims for damages, time or any other impacts based on the documents are expressly waived.

- R\_01 - Historic Plans
- R\_02 - Concept Plans
- R\_03 - Bridge Inspection Reports
- R\_04 - Existing Bridge Plans
- R\_05 - East Selmon Planning and Feasibility Study
- R\_06 - Geotechnical Data
- R\_07 - Type 1 Categorical Exclusion (Ramp 3) (PENDING)
- R\_08 - Non Major State Action (Ramp 2) (PENDING)
- R\_09 - Survey Data
- R\_10 – Selmon Expressway Connector MOU
- R\_11 – Selmon Expressway and I-75 Interchange MOU
- R\_12 – Pipe Video Inspection – Vicinity of Ramp 3
- R\_13 – Utility Information
- R\_14 – Conceptual Typical Sections

**Section V.I. is deleted and replaced with the following:**

**I. Structure Plans**

**1. Bridge Design Analysis:**

- a. The Design-Build Firm shall submit to the Authority final signed and sealed design documentation prepared during the development of the plans.
- b. The Design-Build Firm shall insure that the final geotechnical and hydraulic recommendations and reports required for bridge design are submitted with the 90% bridge plans.
- c. The Design-Build Firm shall "Load Rate" all bridges in accordance with the Department Procedure 850-010-035 and the Structures Manual. The Bridge Load Rating Calculations, the Completed Bridge Load Rating Summary Detail Sheet, and the Load Rating Summary Form shall be submitted to the Authority for review with the 90% superstructure submittal. The final Bridge Load Rating Summary Sheet and Load Rating Summary Form shall be submitted to the Authority for review with the Final superstructure submittal. A final, signed and sealed Bridge Load Rating, updated for as-built conditions, shall be submitted to the Authority for each phase of the bridge construction prior to placing traffic on the completed phase of the bridge. A final, signed and sealed Bridge Load Rating, updated for the as-built conditions as part of the As-Built Plans submittal shall be submitted to the Authority before any traffic is placed on the bridge. The Bridge Load Rating shall be signed and sealed by a Professional Engineer licensed in the State of Florida.
- d. not used.
- e. Any erection, demolition, and any proposed sheeting and/or shoring plans that may potentially impact the railroad must be submitted to and approved by the railroad. This applies to areas adjacent to, within and over railroad rights of ways.
- f. The Engineer of Record for bridges shall analyze the effects of the construction related loads on the permanent structure. These effects include but are not limited to: construction equipment loads, change in segment length, change in construction sequence, etc. The Engineer of Record shall review all specialty engineer submittals (camber curves, falsework systems, etc.) to ensure compliance with the contract plan requirements and intent.

**2. Criteria**

The Design-Build Firm shall incorporate the following into the design of this facility:

- a. All plans and designs are to be prepared in accordance with the Governing Regulations of Section V. A.
- b. Bridge Widening: In general, match the existing as per the Department's Structures Manual.
- c. Critical Temporary Retaining Walls: Whenever the construction of a component requires excavation that may endanger the public or an existing structure that is in use the Design-Build Firm must protect the existing facility and the public. If a critical temporary retaining wall is, therefore, required during the construction stage only, it may be removed and reused after completion of the work. Such systems as steel sheet pilings, soldier beams

and lagging or other similar systems are commonly used. In such cases, the Design-Build Firm is responsible for designing and detailing the wall in the set of contract plans. These plans must be signed and sealed by the Structural Engineer in responsible charge of the wall design.

- d. Partial height walls, including (but not limited to) perched and toe-walls shall not be allowed.
- e. The following environmental classifications shall be utilized for all bridge sites within this project, including Ramp 2 and Ramp 3:

Superstructure Environmental Classification	Concrete Substructure Environmental Classification	Steel Substructure Environmental Classification
Extremely Aggressive	Extremely Aggressive	Extremely Aggressive

3. **Aesthetic Guidelines**

- a. Exposed surfaces of galvanized overhead sign structures shall be painted with Pro-Tech PT211W57 (Textured White). The mating surfaces shall not be painted.
- b. Overhead sign structures shall be monotube structures with mitered corners matching the aesthetic of the existing monotube structures along the East Selmon Expressway.
- c. A Class V surface finish matching the aesthetics of the East Selmon Expressway is required on all new bridges and noise, perimeter and retaining walls, as applicable.

**Respondents MUST** acknowledge receipt of this Addendum/Letter of Clarification by signing, dating and returning the completed Acknowledgement of Receipt of Letter of Addendum/Clarification form **with Respondent’s proposal**.

All other items, conditions, and specifications in the procurement document not specifically changed by the Addendum remain unchanged.

Please send all questions to THEA’s Procurement Manager, Man Le, via email at [Man.Le@tampa-xway.com](mailto:Man.Le@tampa-xway.com).

**ACKNOWLEDGEMENT OF RECEIPT OF ADDENDUM and/or LETTER OF CLARIFICATION**

Were Addenda issued on this Solicitation?

- Yes
- No

Were Letter of Clarification issued on this Solicitation?

- Yes
- No

I (We) hereby acknowledge receipt of the following Addendum/Addenda issued in reference to this solicitation by listing the Addenda by number, date and signing the form:

Addendum \_\_\_\_\_

Date: \_\_\_\_\_

Addendum \_\_\_\_\_

Date: \_\_\_\_\_

Letter of Clarification \_\_\_\_\_

Date: \_\_\_\_\_

Letter of Clarification \_\_\_\_\_

Date: \_\_\_\_\_

**BIDDER:**

By: \_\_\_\_\_  
Authorized Signature

\_\_\_\_\_  
Printed Name of Signer

\_\_\_\_\_  
Title of Signer

\_\_\_\_\_  
Date Signed

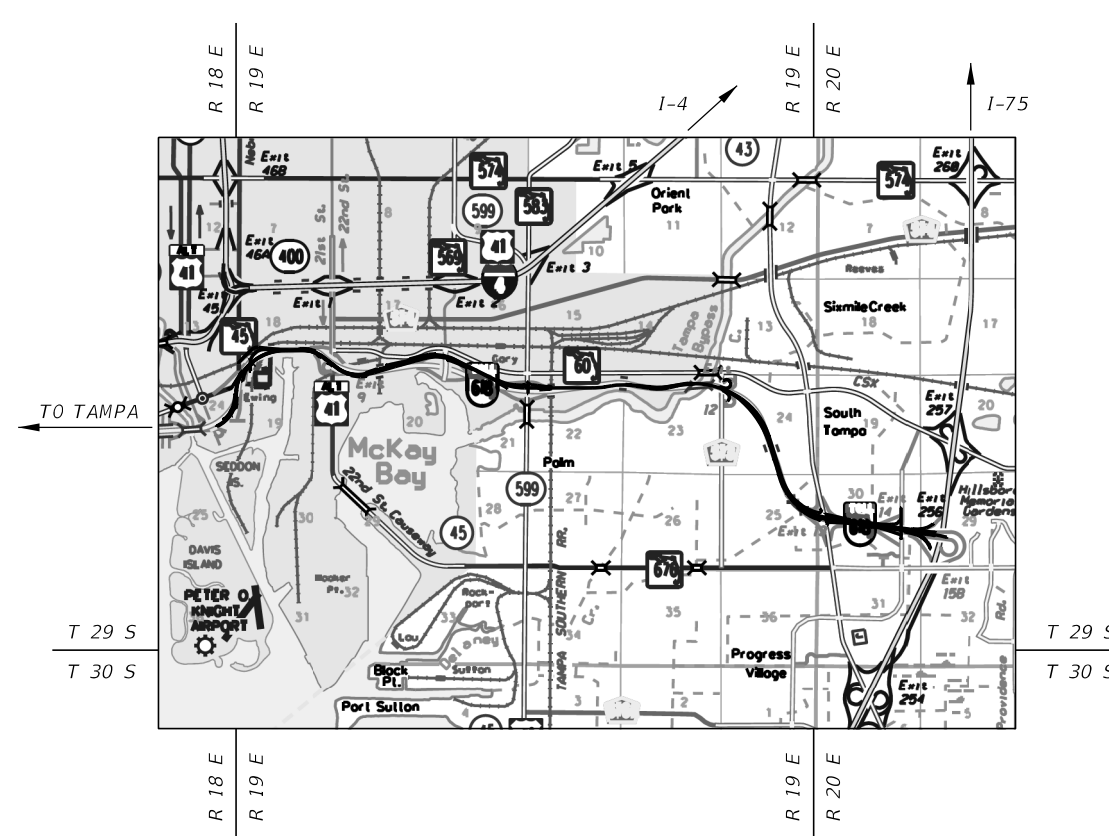
**[END OF ACKNOWLEDGMENT OF RECEIPT FORM]**

TAMPA HILLSBOROUGH EXPRESSWAY AUTHORITY

SELMON EAST SLIP RAMPS DESIGN-BUILD PROJECT

HILLSBOROUGH COUNTY (10002)

STATE ROAD NO. 618



TYPICAL SECTIONS

SHEET NO.	SHEET DESCRIPTION
1	COVER SHEET
2	BRIDGE NO. 100447 SELMON EXPRESSWAY OVER CSX RR
3	BRIDGE NO. 100489 & 100490 OVER S. FALKENBURG RD
4	SR 618 SLIP RAMP 3 ONTO REVERSIBLE EXPRESS LANES

REVISED MARCH 1, 2021

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL              ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

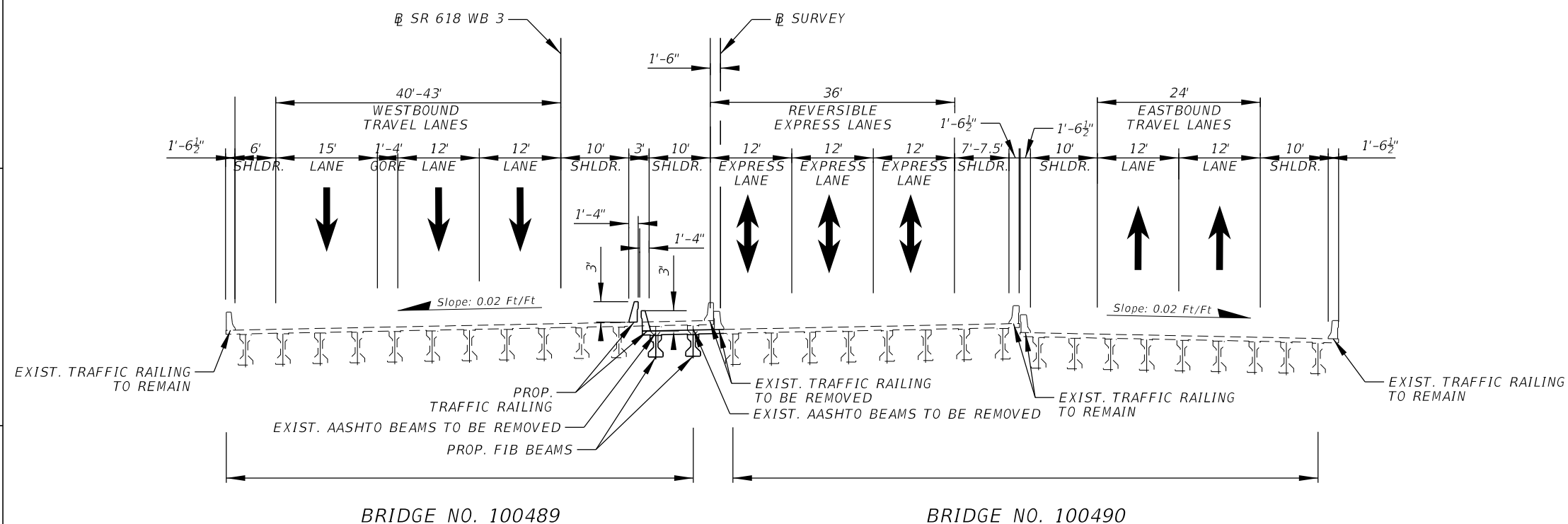
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



BRIDGE NO. 100489 & 100490  
OVER S FALKENBURG RD

NOT TO SCALE

FINANCIAL PROJECT ID	SHEET NO.

REVISION 03/01/2021

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

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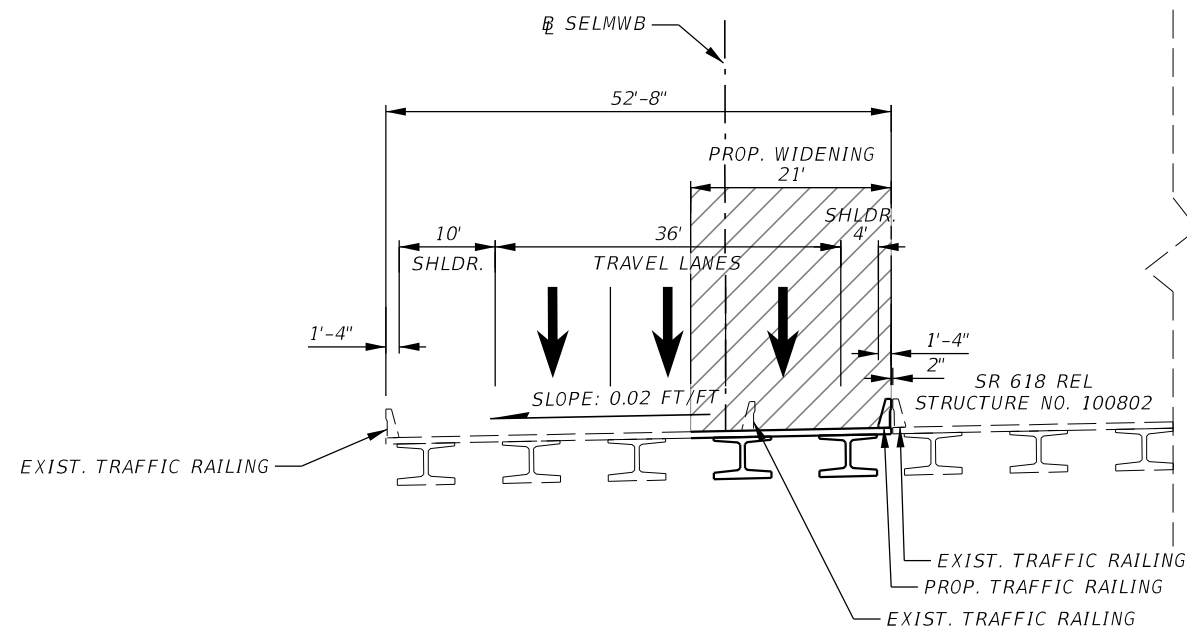
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POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



**BRIDGE NO. 100447  
SELMON EXPRESSWAY OVER CSX RR**

NOT TO SCALE

FINANCIAL PROJECT ID	SHEET NO.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

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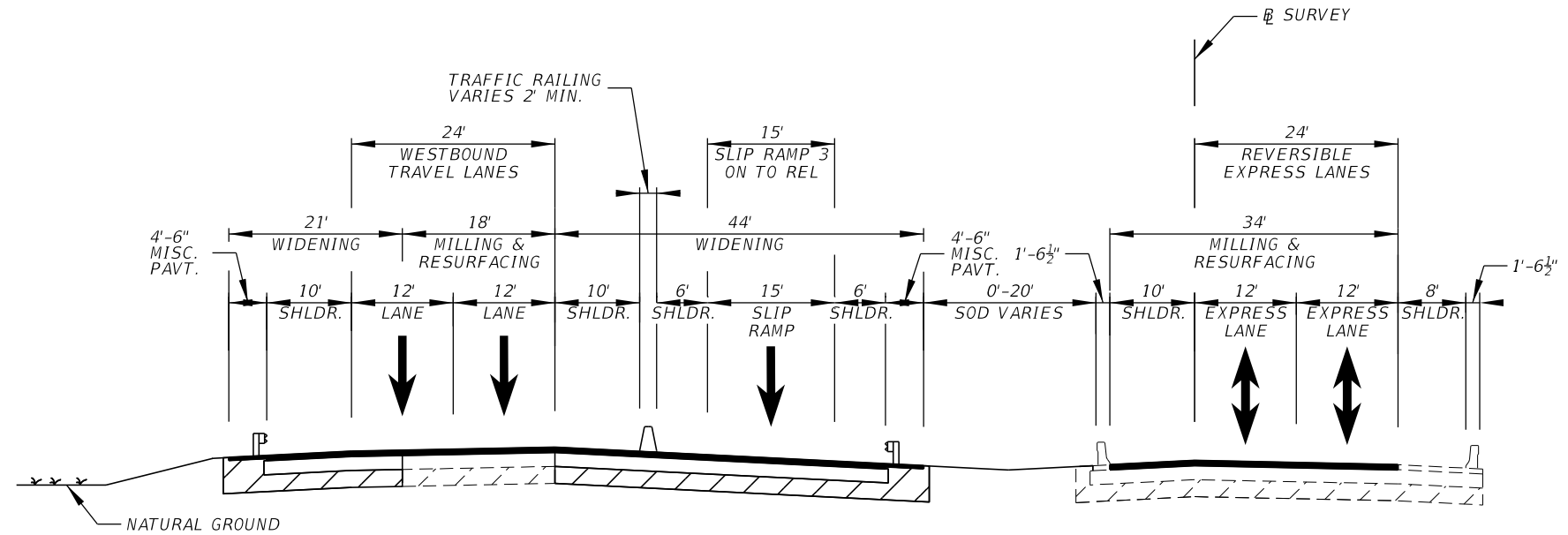
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POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



SR 618 SLIP RAMP 3  
ONTO REVERSIBLE EXPRESS LANES

NOT TO SCALE

FINANCIAL PROJECT ID	SHEET NO.

STATE OF FLORIDA  
TAMPA HILLSBOROUGH EXPRESSWAY AUTHORITY

TYPICAL SECTION PACKAGE

THEA PROJECT NO. 017011-1-6

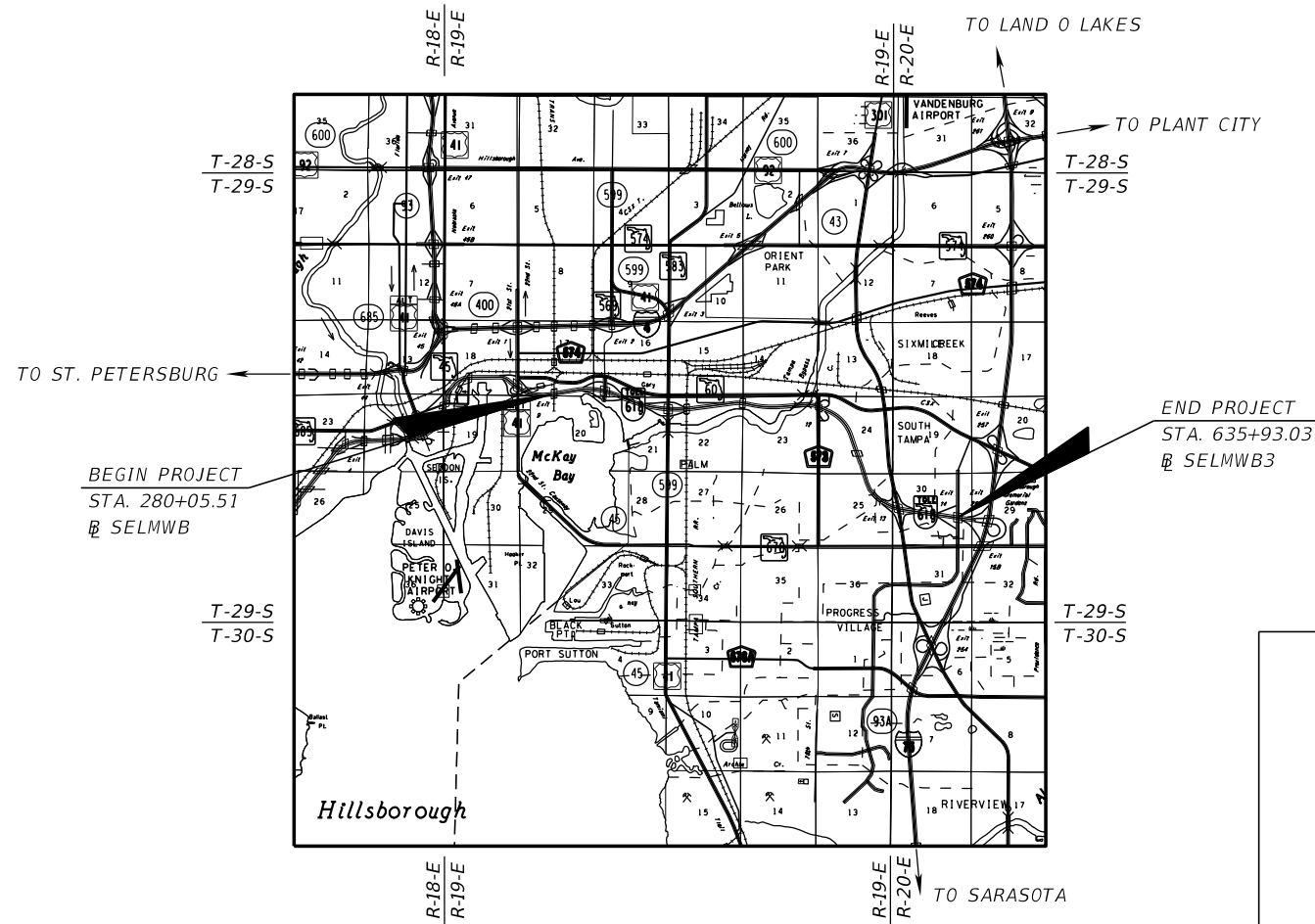
HILLSBOROUGH COUNTY

STATE ROAD NO. 618

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE WITH RULE 61G15-23.004, F.A.C.

TYPICAL SECTION PACKAGE

SHEET NO	SHEET DESCRIPTION
1-1 to 1-4	PHASE 1 ROADWAY MAINLINE TYPICAL SECTIONS
1-5 to 1-16	PHASE 1 BRIDGE MAINLINE TYPICAL SECTIONS
1-17 to 1-24	PHASE 1 BRIDGE ELEVATIONS
1-25 to 1-27	PHASE 1 ROADWAY RAMP TYPICAL SECTIONS
1-28 to 1-29	PHASE 1 ROADWAY NEW CONSTRUCTION TYPICAL SECTIONS
2-1 to 2-6	PHASE 2 ROADWAY MAINLINE TYPICAL SECTIONS
2-7 to 2-21	PHASE 2 BRIDGE MAINLINE TYPICAL SECTIONS
2-22 to 2-25	PHASE 2 ROADWAY RAMP TYPICAL SECTIONS
2-26	PHASE 2 ROADWAY NEW CONSTRUCTION TYPICAL SECTIONS
3-1	PHASE 3 ROADWAY MAINLINE TYPICAL SECTIONS
3-2 to 3-4	PHASE 3 BRIDGE MAINLINE TYPICAL SECTIONS
3-5 to 3-15	PHASE 3 BRIDGE ELEVATIONS
4-1	PHASE 4 ROADWAY RAMP TYPICAL SECTIONS
4-2	PHASE 4 BRIDGE ELEVATIONS
4-3	PHASE 4 BRIDGE MAINLINE TYPICAL SECTIONS



TYPICAL SECTION CONCURRENCE		
FDOT DISTRICT DESIGN ENGINEER	FDOT DISTRICT STRUCTURES DESIGN ENGINEER	FHWA TRANSPORTATION ENGINEER

DESIGN SPEED AND POSTED SPEED CONCURRENCE:	CONTEXT CLASSIFICATION CONCURRENCE:

FDOT DISTRICT TRAFFIC OPERATIONS ENGINEER	FDOT DISTRICT DESIGN ENGINEER	FDOT DISTRICT INTERMODAL SYSTEMS DEVELOPMENT MANAGER
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SHEET NO.

1-A

# PHASE 1

DATE		DESCRIPTION		REVISIONS		TAMPA HILLSBOROUGH EXPRESSWAY AUTHORITY			SHEET NO.
DATE	DESCRIPTION	DATE	DESCRIPTION	ROAD NO.	COUNTY	FINANCIAL PROJECT ID			
				618	HILLSBOROUGH				1
WSP USA Inc. 2202 North West Shore Blvd., Suite 300 Tampa, Florida 33607 (813) 520-4444 Cert. of Auth. No. 1462									

**PROJECT CONTROLS**

**CONTEXT CLASSIFICATION**

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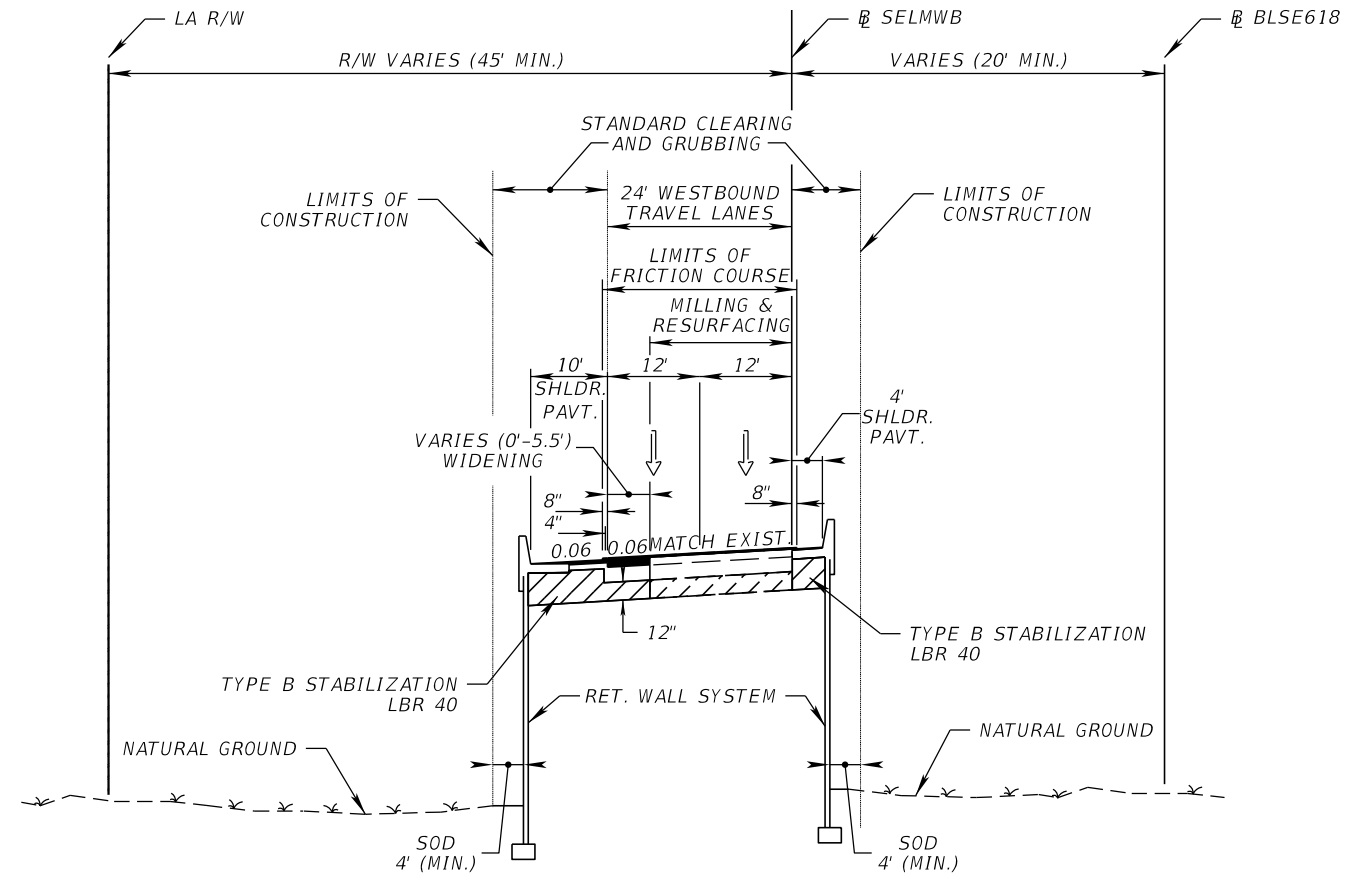
**CRITERIA**

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

**POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:**

VARIATIONS: SHOULDER WIDTH  
 EXCEPTIONS: SHOULDER WIDTH, LANE WIDTH

**TYPICAL SECTION No.**



**TYPICAL SECTION**  
**2-LANE**  
**NTS**  
 DESIGN SPEED 60

EXCLUDING BRIDGES NO. 100447, 100449

**SELMON EXPRESSWAY WESTBOUND**

CURRENT YEAR = 2016 AADT = 35,300  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 60,700  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

FINANCIAL PROJECT ID	SHEET NO.
	1-1

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

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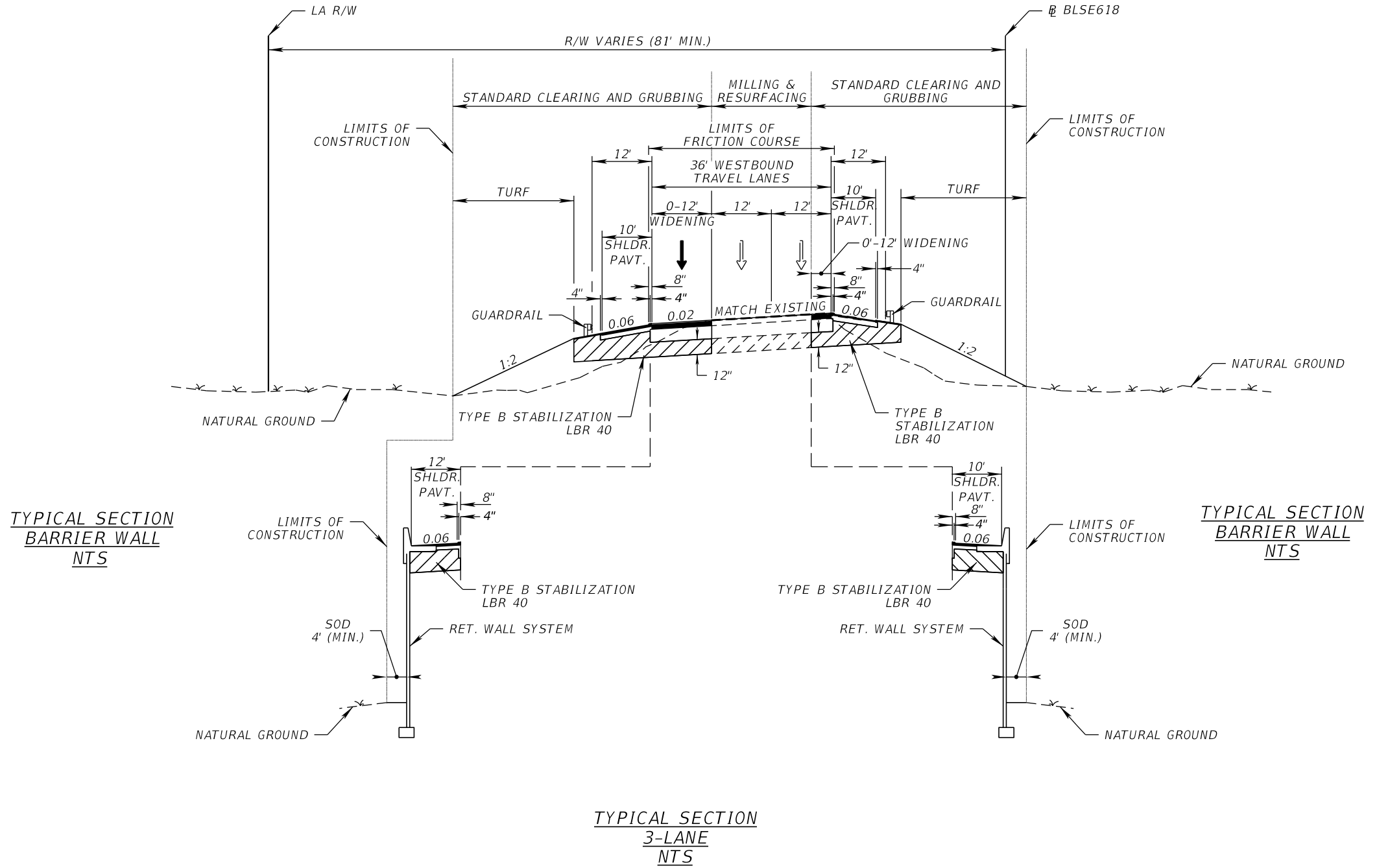
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- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

VARIATIONS: SHOULDER WIDTH, HORIZONTAL STOPPING SIGHT DISTANCE

**TYPICAL SECTION No.**



**SELMON EXPRESSWAY WESTBOUND**

CURRENT YEAR = 2016 AADT = 35,300  
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 ESTIMATED DESIGN YEAR = 2040 AADT = 60,700  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

**TYPICAL SECTION 3-LANE**  
 NTS  
 DESIGN SPEED 60

EXCLUDING BRIDGES NO. 100453, 100455, 100457, 100459, 100465

FINANCIAL PROJECT ID	SHEET NO.
	1-2

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

**CONTEXT CLASSIFICATION**

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- ( ) MINOR ARTERIAL
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- ( ) MINOR COLLECTOR
- ( ) LOCAL

**HIGHWAY SYSTEM**

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**CRITERIA**

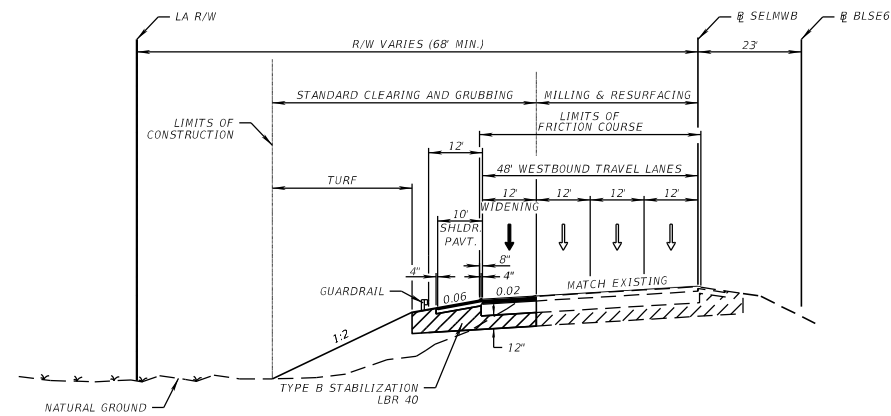
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- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

**POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:**

VARIATIONS: SHOULDER WIDTH, HORIZONTAL STOPPING SIGHT DISTANCE

EXCEPTIONS: SHOULDER WIDTH, STOPPING SIGHT DISTANCE

**TYPICAL SECTION No.**

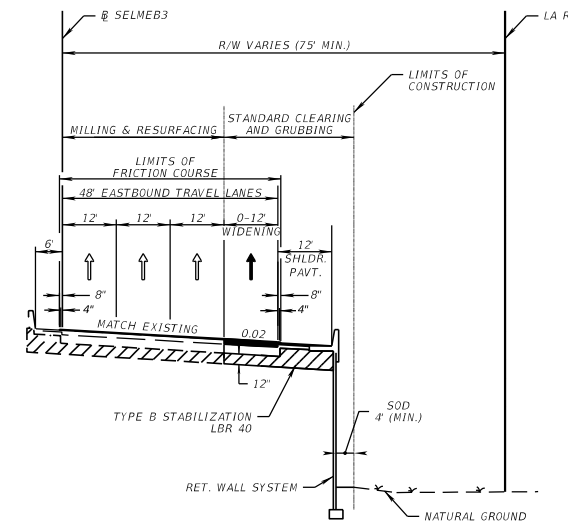


**TYPICAL SECTION  
4-LANE  
NTS  
DESIGN SPEED 60**

EXCLUDING BRIDGES NO. 100836, 100807,  
100809, 100489

**SELMON EXPRESSWAY WESTBOUND**

CURRENT YEAR = 2016 AADT = 35,300  
ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
ESTIMATED DESIGN YEAR = 2040 AADT = 60,700  
K = 10% D = 65% T = 3% (24 HOUR)  
DESIGN HOUR T = 3%



**TYPICAL SECTION  
4-LANE  
NTS  
DESIGN SPEED 60**

EXCLUDING BRIDGES NO. 100808

**SELMON EXPRESSWAY EASTBOUND**

CURRENT YEAR = 2016 AADT = 32,500  
ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
ESTIMATED DESIGN YEAR = 2040 AADT = 64,500  
K = 10% D = 65% T = 3% (24 HOUR)  
DESIGN HOUR T = 3%

FINANCIAL PROJECT ID	SHEET NO.
	1-3

**PROJECT CONTROLS**

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- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

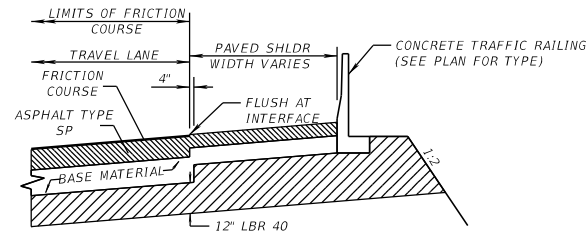
CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

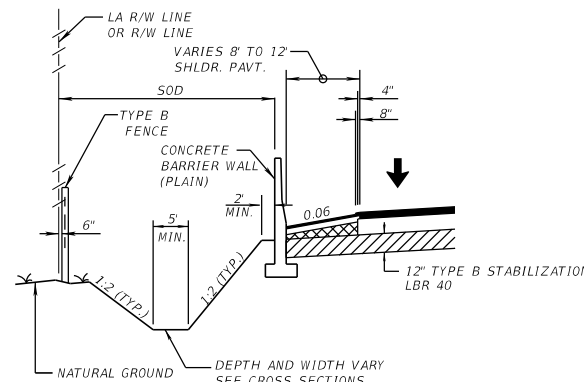
**TYPICAL SECTION No.**

SHOULDER TREATMENT DETAILS

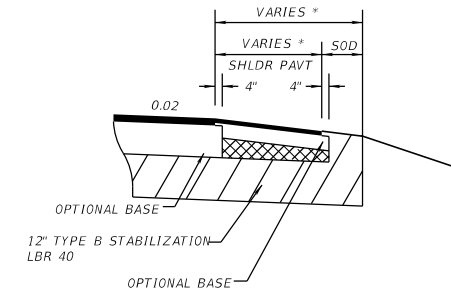


PROVIDE A 25' LONG VERTICAL TRANSITION OF THE SHOULDER PAVEMENT STRUCTURAL SECTION FROM 0.75' BELOW THE TOP OF THE HIGH SIDE FRICTION COURSE, TO FLUSH WITH THE TOP OF THE FRICTION COURSE. END THE VERTICAL SHOULDER PAVEMENT TRANSITION WHEN THE HIGH SIDE SHOULDER PAVEMENT CROSS SLOPE EQUALS 0% AND KEEP IT FLUSH WITH THE FRICTION COURSE THROUGH THE FULL SUPER DISTANCE.

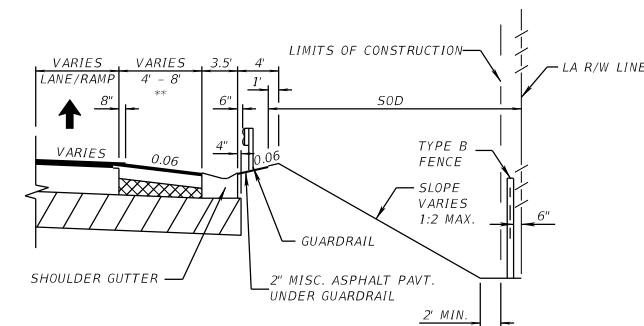
SHOULDER PAVING ON HIGH SIDE OF SUPER ELEVATED ROADWAYS  
NTS



SHOULDER WITH BARRIER WALL (RIGID SHOULDER)  
MAINLINE AND RAMPS  
NTS



SHOULDER PAVEMENT DETAIL  
NTS  
\*SEE PLAN FOR SPECIFIC WIDTHS



\*\* SHOULDER PAVEMENT LESS THAN 6' IN WIDTH THAT ADJOINS SHOULDER GUTTER, SHALL BE THE SAME TYPE, DEPTH AND SLOPE AS THE RAMP PAVEMENT.

SHOULDER WITH SHOULDER GUTTER  
MAINLINE AND RAMPS  
NTS

FINANCIAL PROJECT ID	SHEET NO.
	1-4

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN              ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.          ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                      ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.              ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL          ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

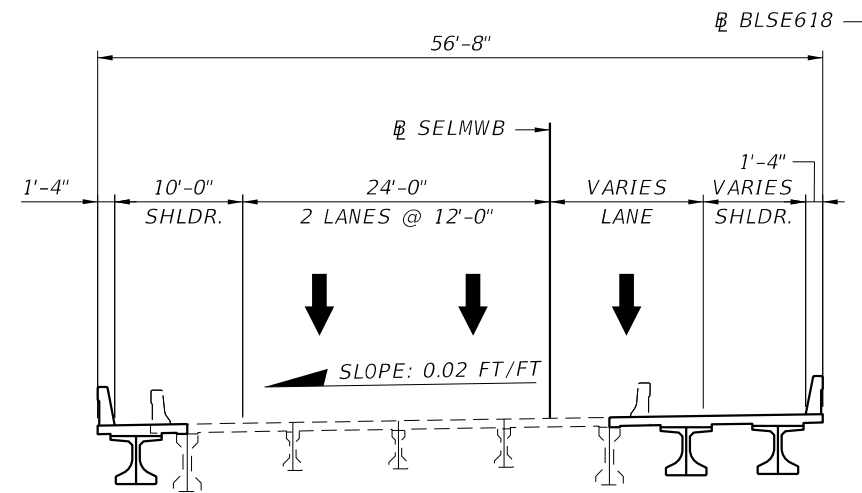
CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

VARIATIONS: SHOULDER WIDTH

**TYPICAL SECTION No.**



**BRIDGE NO. 100447**  
**SELMON EXPRESSWAY OVER CSX RAILROAD**  
NTS

**SELMON EXPRESSWAY WESTBOUND**

CURRENT YEAR = 2016 AADT = 35,300  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 60,700  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

CONCRETE REMOVAL

FINANCIAL PROJECT ID	SHEET NO.
	1-5

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL              ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

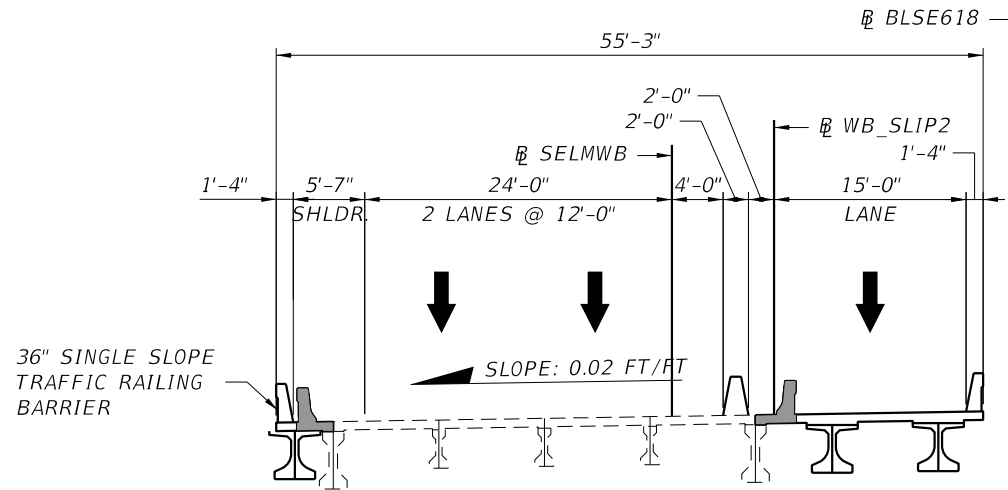
CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

EXCEPTIONS: SHOULDER WIDTH

**TYPICAL SECTION No.**



**BRIDGE NO. 100449**  
**SELMON EXPRESSWAY OVER 34TH STREET**  
**NTS**

**SELMON EXPRESSWAY WESTBOUND**

CURRENT YEAR = 2016 AADT = 35,300  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 60,700  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

CONCRETE REMOVAL

FINANCIAL PROJECT ID	SHEET NO.
	1-6

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN              ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.          ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                      ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.              ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL          ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

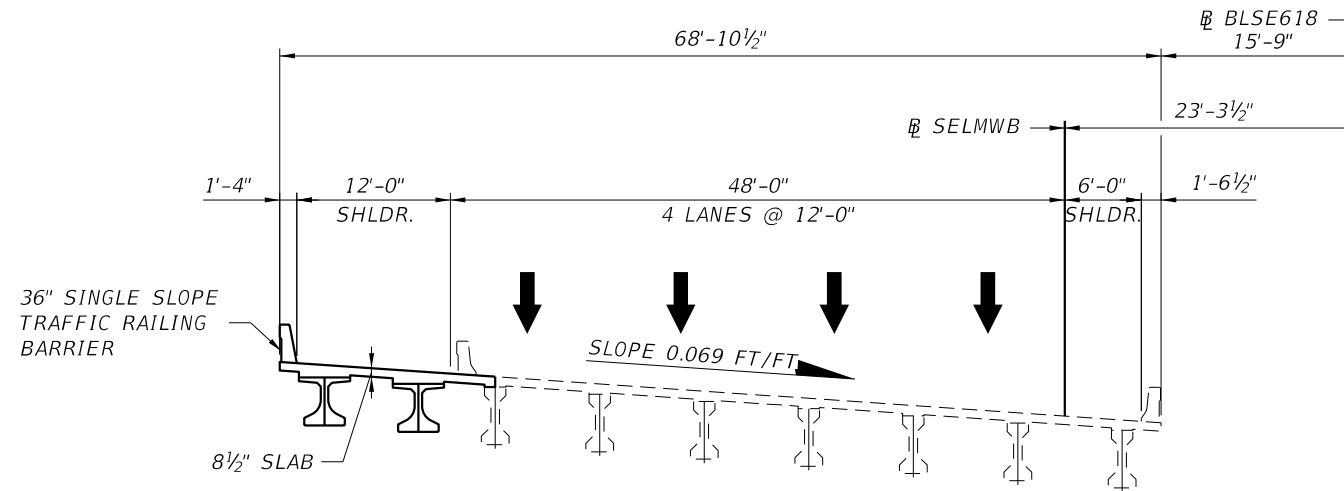
CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

EXCEPTIONS: SHOULDER WIDTH

**TYPICAL SECTION No.**



**BRIDGE NO. 100836**  
**SELMON EXPRESSWAY OVER 39TH STREET**  
**NTS**

**SELMON EXPRESSWAY WESTBOUND**

CURRENT YEAR = 2016 AADT = 35,300  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 60,700  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

CONCRETE REMOVAL

FINANCIAL PROJECT ID	SHEET NO.
	1-7

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                      ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL              ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

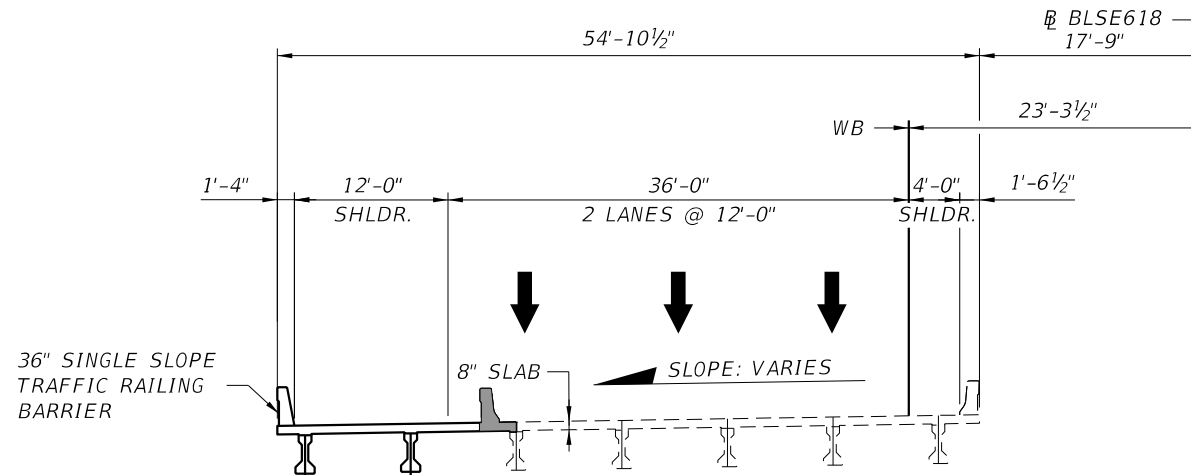
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



**BRIDGE NO. 100453**  
**SELMON EXPRESSWAY OVER 50TH STREET**  
**NTS**

**SELMON EXPRESSWAY WESTBOUND**

CURRENT YEAR = 2016 AADT = 35,300  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 60,700  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

CONCRETE REMOVAL

FINANCIAL PROJECT ID	SHEET NO.
	1-8

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN            ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.        ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                      ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.            ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL        ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

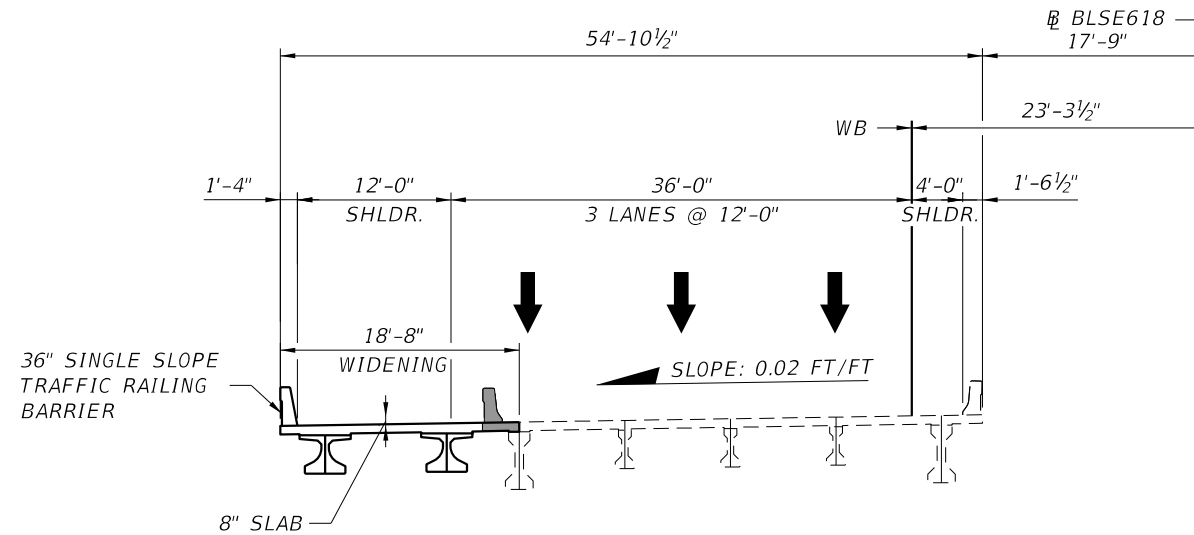
CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

EXCEPTIONS: VERTICAL CLEARANCE

**TYPICAL SECTION No.**



**BRIDGE NO. 100455**  
**SELMON EXPRESSWAY OVER SCL RAILROAD**  
**NTS**

**SELMON EXPRESSWAY WESTBOUND**

CURRENT YEAR = 2016 AADT = 35,300  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 60,700  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

CONCRETE REMOVAL

FINANCIAL PROJECT ID	SHEET NO.
	1-9

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL            ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL            (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN    ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES. ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE            ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.    ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

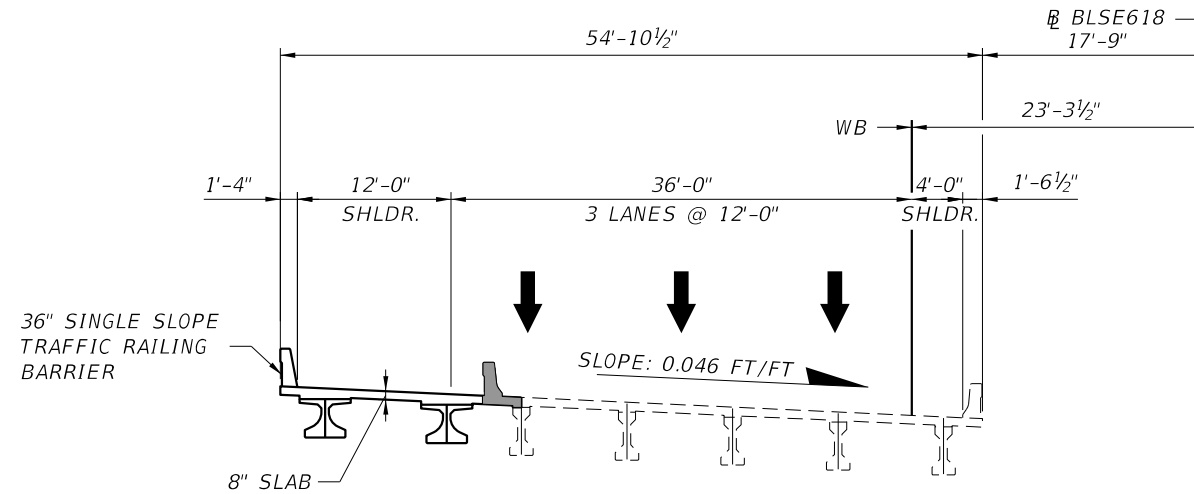
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



**BRIDGE NO. 100457**  
**SELMON EXPRESSWAY OVER MAYDELL DRIVE**  
**NTS**

**SELMON EXPRESSWAY WESTBOUND**

CURRENT YEAR = 2016 AADT = 35,300  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 60,700  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

CONCRETE REMOVAL

FINANCIAL PROJECT ID	SHEET NO.
	1-10

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**PROJECT CONTROLS**

**CONTEXT CLASSIFICATION**

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN              ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.        ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

**FUNCTIONAL CLASSIFICATION**

- ( ) INTERSTATE                      ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.              ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL          ( ) LOCAL
- ( ) MINOR ARTERIAL

**HIGHWAY SYSTEM**

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

**ACCESS CLASSIFICATION**

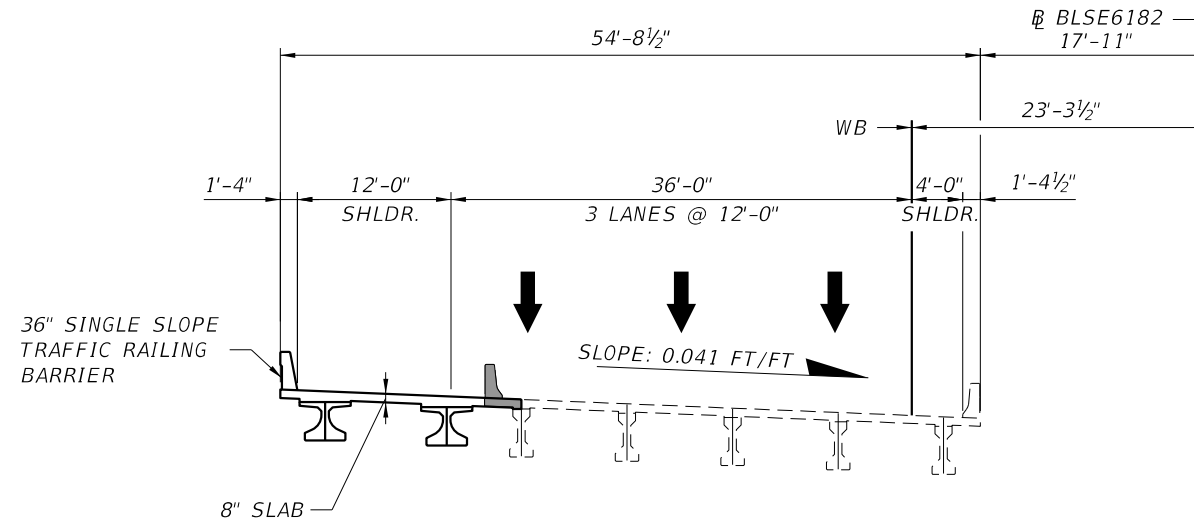
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

**CRITERIA**

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

**POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:**

**TYPICAL SECTION No.**



**BRIDGE NO. 100459**  
**SELMON EXPRESSWAY OVER BYPASS CANAL**  
**NTS**

**SELMON EXPRESSWAY WESTBOUND**

CURRENT YEAR = 2016 AADT = 35,300  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 60,700  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

CONCRETE REMOVAL

FINANCIAL PROJECT ID	SHEET NO.
	1-11

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL            ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL            (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN    ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES. ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE            ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.    ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

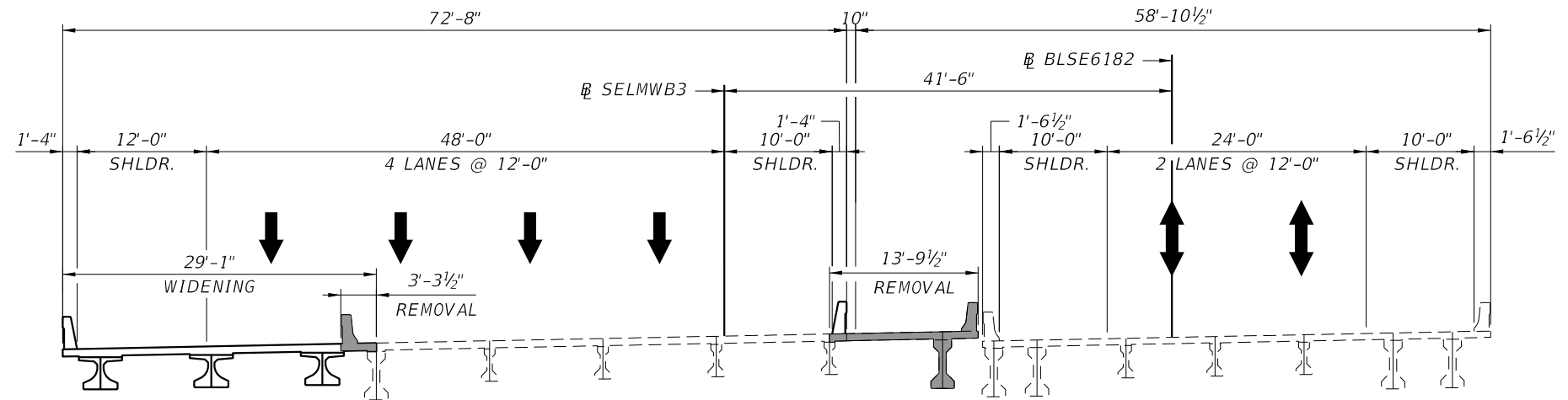
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



**BRIDGE NO. 100807**  
**SELMON EXPRESSWAY OVER PALM RIVER ROAD**  
**NTS**

**SELMON EXPRESSWAY EASTBOUND**

CURRENT YEAR = 2016 AADT = 32,500  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 64,500  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

**SELMON EXPRESSWAY WESTBOUND**

CURRENT YEAR = 2016 AADT = 35,300  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 60,700  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

CONCRETE REMOVAL

FINANCIAL PROJECT ID	SHEET NO.
	1-12

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN              ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.          ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                      ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.              ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL          ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

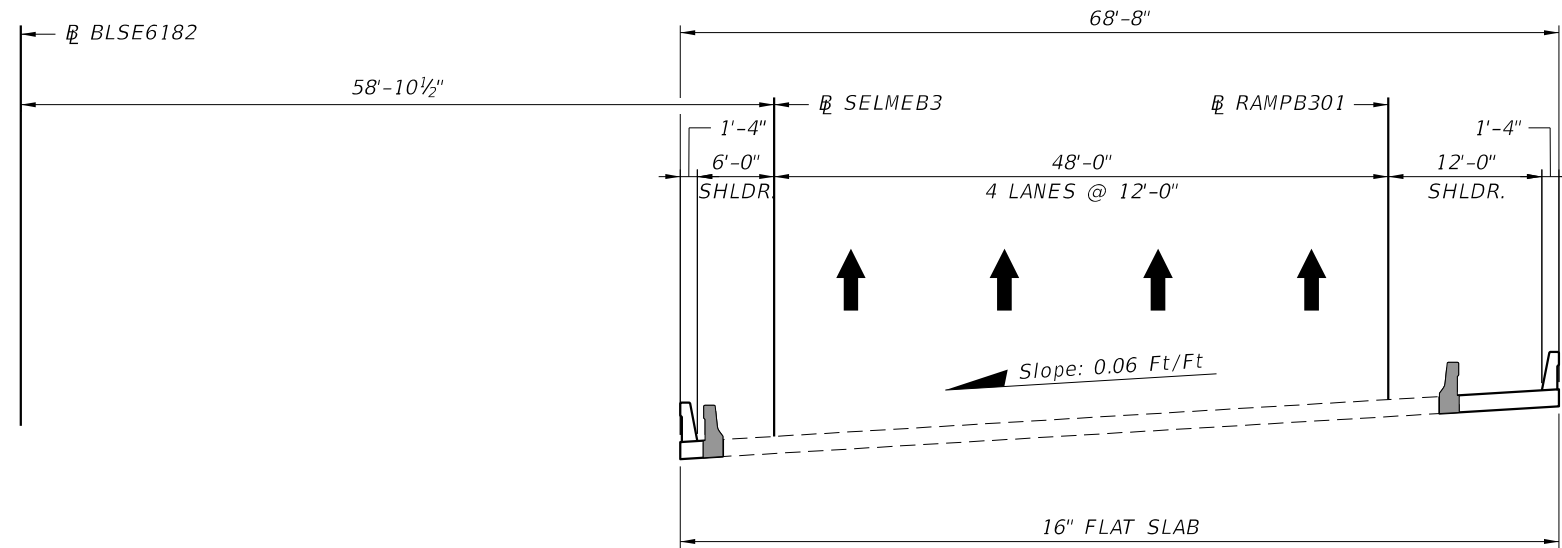
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



**BRIDGE NO. 100808**  
**SELMON EXPRESSWAY OVER DELANEY CREEK**  
**NTS**

**SELMON EXPRESSWAY EASTBOUND**

CURRENT YEAR = 2016 AADT = 32,500  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 64,500  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

CONCRETE REMOVAL

FINANCIAL PROJECT ID	SHEET NO.
	1-13

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**PROJECT CONTROLS**

**CONTEXT CLASSIFICATION**

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN              ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.          ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

**FUNCTIONAL CLASSIFICATION**

- ( ) INTERSTATE                      ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL            ( ) LOCAL
- ( ) MINOR ARTERIAL

**HIGHWAY SYSTEM**

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

**ACCESS CLASSIFICATION**

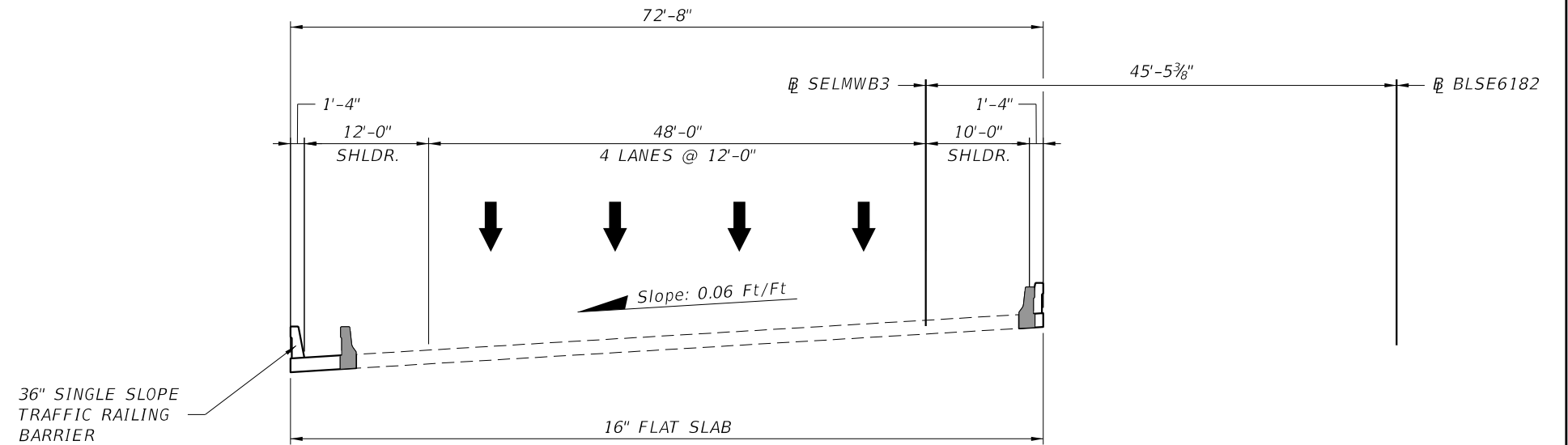
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

**CRITERIA**

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

**POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:**

**TYPICAL SECTION No.**



**BRIDGE NO. 100809**  
**SELMON EXPRESSWAY OVER DELANEY CREEK**  
**NTS**

**SELMON EXPRESSWAY WESTBOUND**

CURRENT YEAR = 2016 AADT = 35,300  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 60,700  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

CONCRETE REMOVAL

FINANCIAL PROJECT ID	SHEET NO.
	1-14

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL            ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL            (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN    ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES. ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE            ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.    ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL   ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

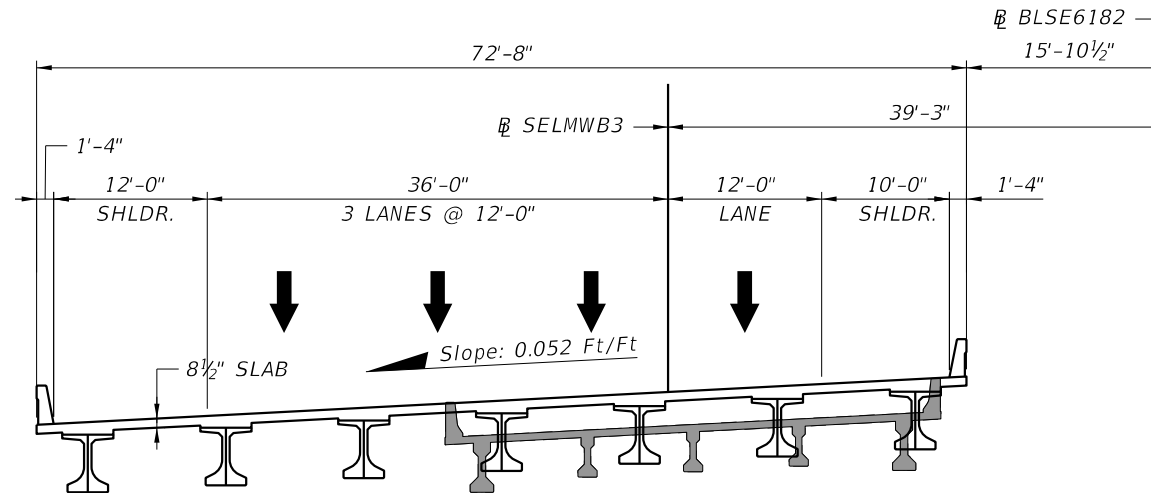
- (x) 1 - FREEWAY
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- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



**BRIDGE NO. 100465**  
**REPLACEMENT - SELMON EXPRESSWAY OVER US 301**  
**NTS**

**SELMON EXPRESSWAY WESTBOUND**

CURRENT YEAR = 2016 AADT = 35,300  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 60,700  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

CONCRETE REMOVAL

FINANCIAL PROJECT ID	SHEET NO.
	1-15

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL              ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

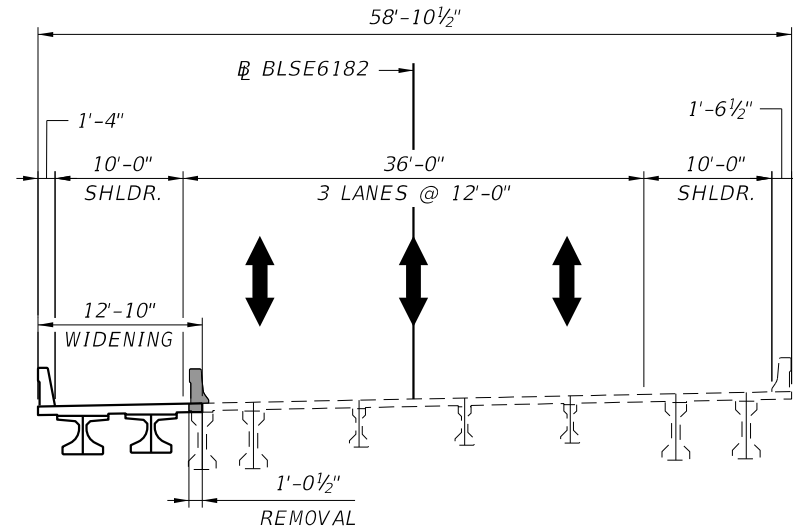
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- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



REL  
BRIDGE NO. 100461  
SELMON EXPRESSWAY OVER PALM RIVER ROAD  
NTS

**SELMON EXPRESSWAY EASTBOUND**

CURRENT YEAR = 2016 AADT = 32,500  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 64,500  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

CONCRETE REMOVAL

FINANCIAL PROJECT ID	SHEET NO.
	2-14

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                 ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL              ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

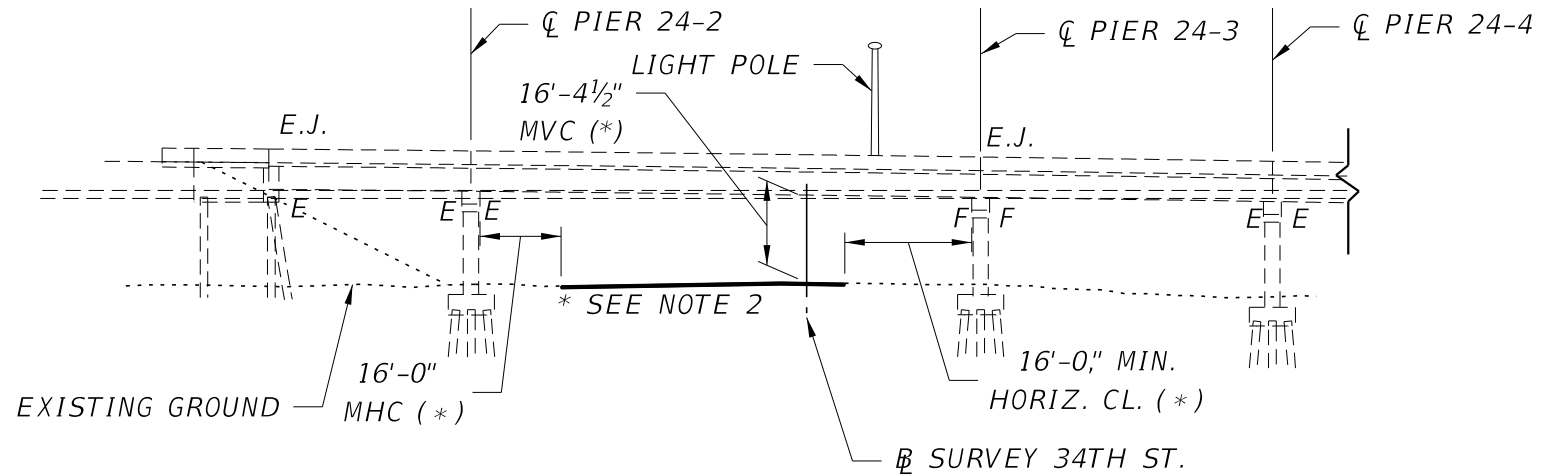
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



**BRIDGE NO. 100449, 100450**  
**SELMON EXPRESSWAY OVER 34TH STREET**  
**NTS**

MHC = MINIMUM HORIZONTAL CLEARANCE  
MVC = MINIMUM VERTICAL CLEARANCE

FINANCIAL PROJECT ID	SHEET NO.
	1-17

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL            ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

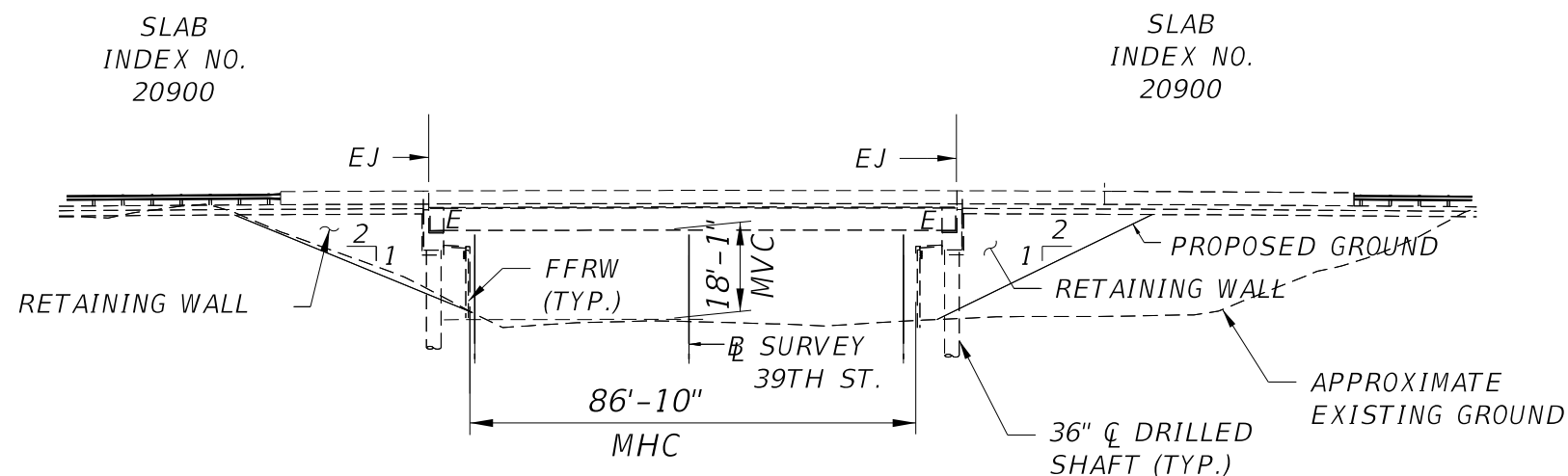
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



**BRIDGE NO. 100805, 100836**  
**SELMON EXPRESSWAY OVER 39TH STREET**  
**NTS**

FINANCIAL PROJECT ID	SHEET NO.
	1-18

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL            ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

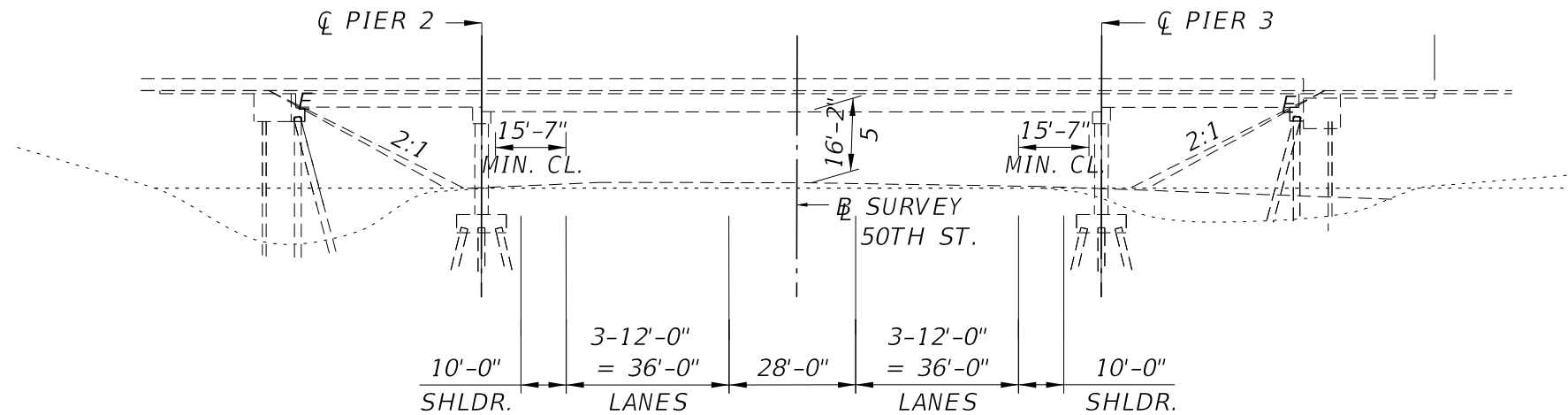
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



**BRIDGE NO. 100453, 100454**  
**SELMON EXPRESSWAY OVER 50TH STREET**  
**NTS**

FINANCIAL PROJECT ID	SHEET NO.
	1-19

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL              ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

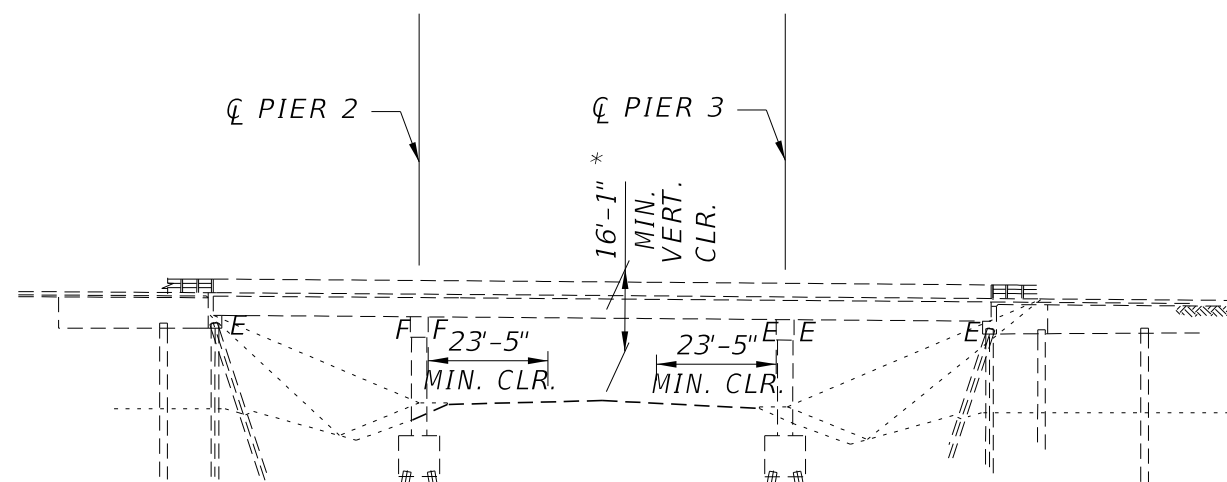
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



**BRIDGE NO. 100457, 100458**  
**SELMON EXPRESSWAY OVER MAYDELL DRIVE**  
**NTS**

\*EXISTING MIN. VERTICAL CLEARANCE WILL BE REDUCED

FINANCIAL PROJECT ID	SHEET NO.
	1-20

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL              ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

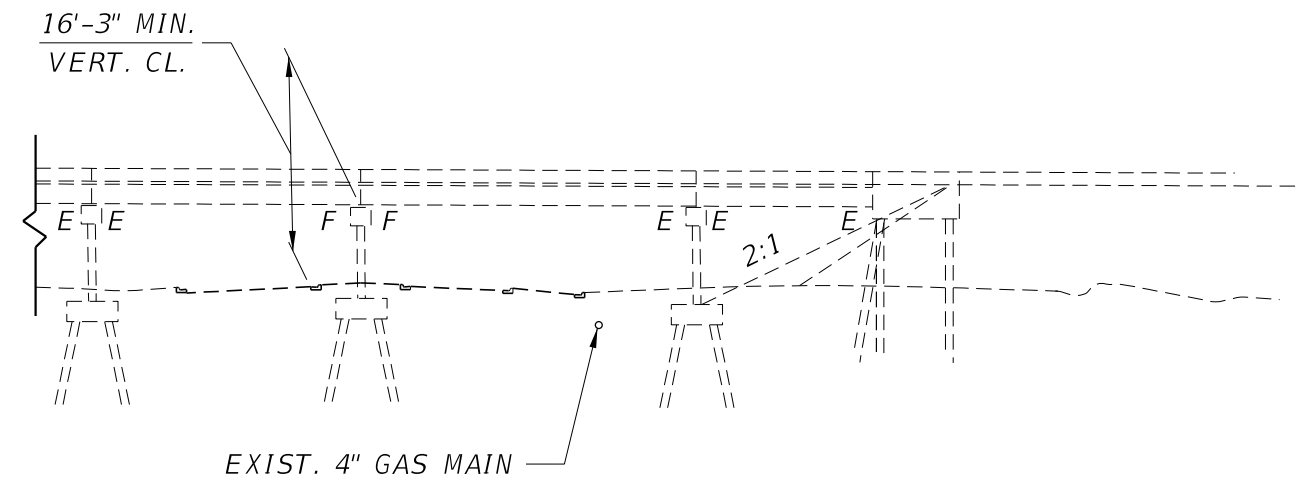
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



**BRIDGE NO. 100459, 100460**  
**SELMON EXPRESSWAY OVER 78TH STREET**  
**NTS**

FINANCIAL PROJECT ID	SHEET NO.
	1-21

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL            ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

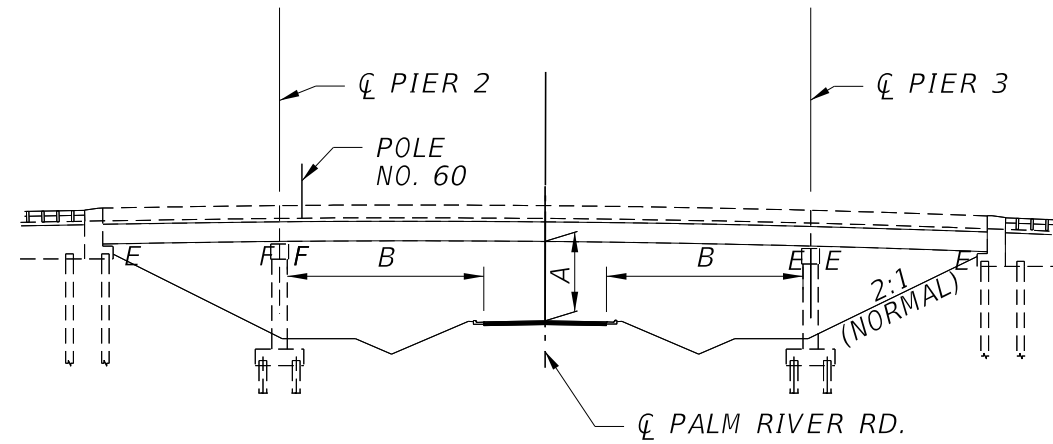
- (x) 1 - FREEWAY
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- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



**BRIDGE NO. 100461, 100462, 100807**  
**SELMON EXPRESSWAY OVER PALM RIVER ROAD**  
**NTS**

\*LOOKING EAST

FINANCIAL PROJECT ID	SHEET NO.
	1-22

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN              ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.          ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                      ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.              ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL          ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

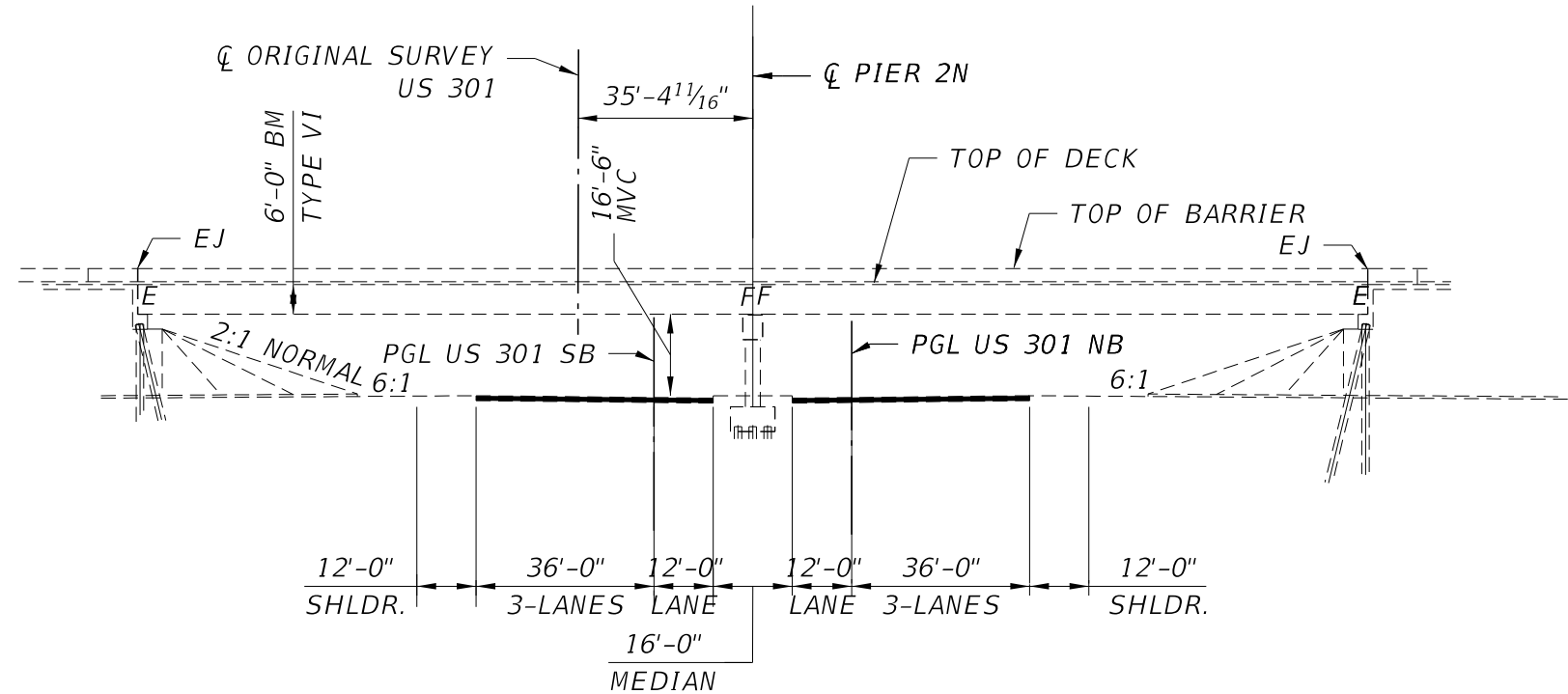
- (x) 1 - FREEWAY
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- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



**BRIDGE NO. 100465, 100466, 100811  
SELMON EXPRESSWAY OVER US 301  
NTS**

FINANCIAL PROJECT ID	SHEET NO.
	1-23

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL            ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

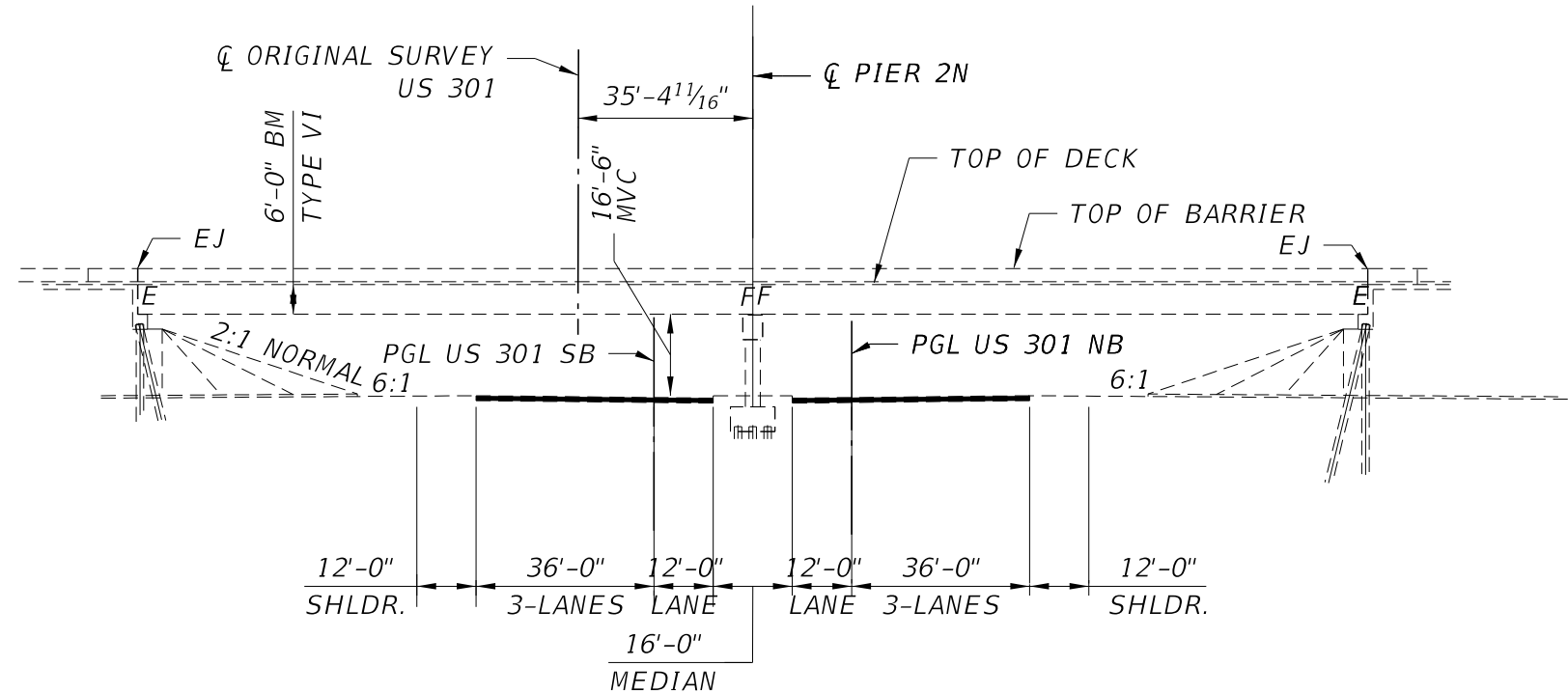
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- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



**BRIDGE NO. 100465, 100466, 100811  
SELMON EXPRESSWAY OVER US 301  
NTS**

FINANCIAL PROJECT ID	SHEET NO.
	1-23

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL            ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL            (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN    ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES. ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE            ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.    ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

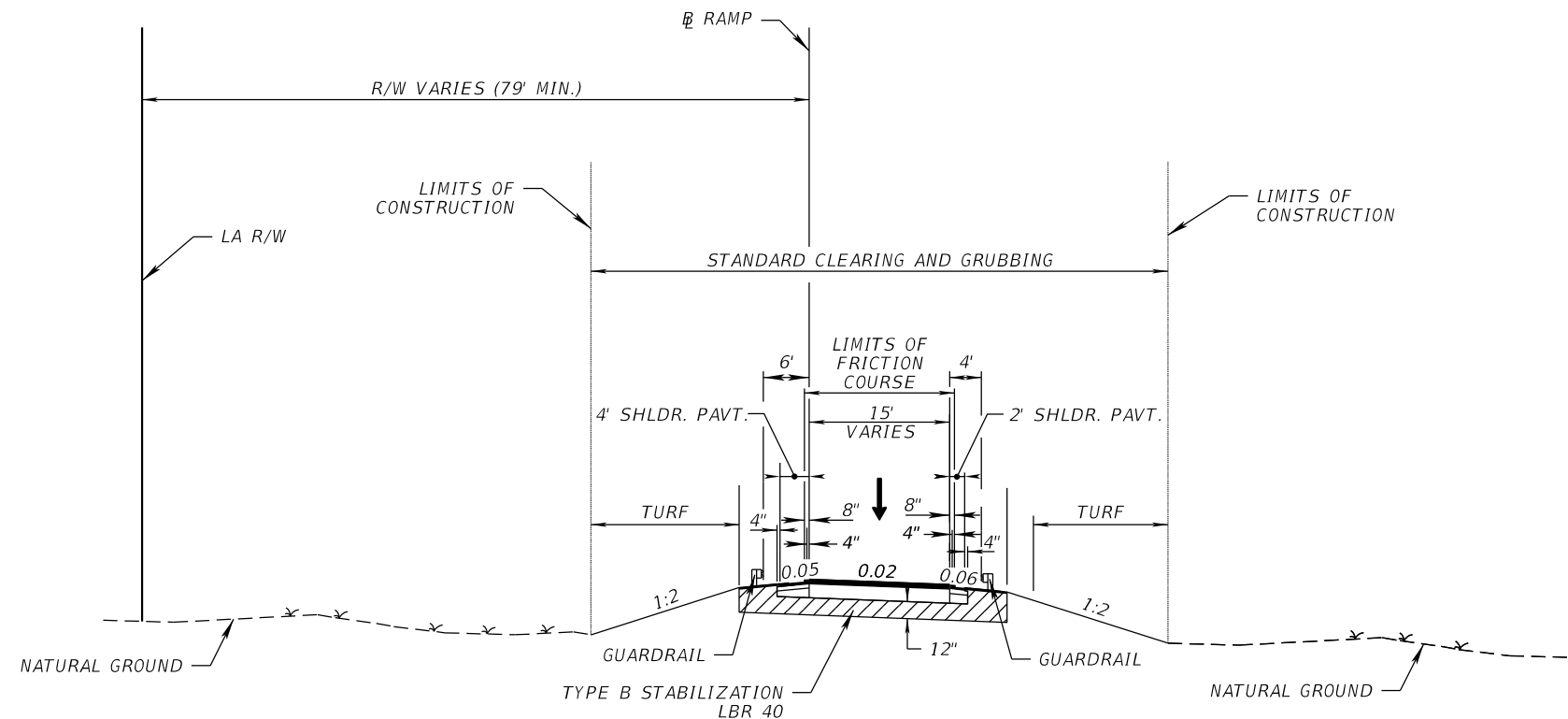
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



**TYPICAL SECTION  
1-LANE RAMP  
NTS**

DESIGN SPEED VARIES:  
20 MPH AT TERMINALS  
30-35 MPH ON LOOP RAMPS  
40-50 MPH AT GORE LOCATIONS

FINANCIAL PROJECT ID	SHEET NO.
	1-25

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                 ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL              ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

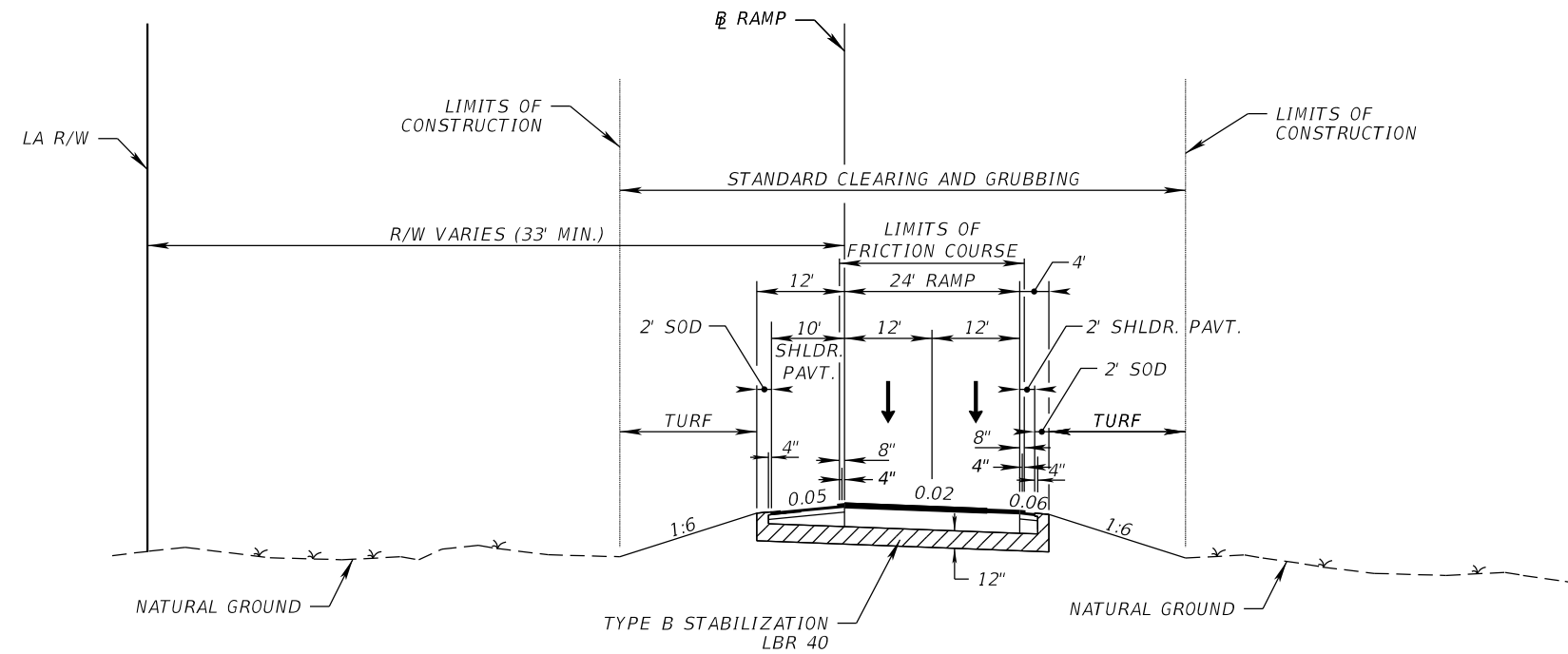
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



**TYPICAL SECTION  
2-LANE RAMP  
NTS**

DESIGN SPEED VARIES:  
20 MPH AT TERMINALS  
30-35 MPH ON LOOP RAMPS  
40-50 MPH AT GORE LOCATIONS

FINANCIAL PROJECT ID	SHEET NO.
	1-26

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

**CONTEXT CLASSIFICATION**

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

**FUNCTIONAL CLASSIFICATION**

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL              ( ) LOCAL
- ( ) MINOR ARTERIAL

**HIGHWAY SYSTEM**

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

**ACCESS CLASSIFICATION**

- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

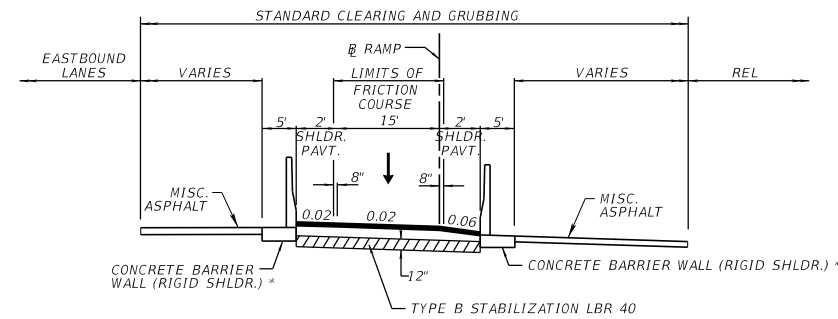
**CRITERIA**

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

**POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:**

EXCEPTIONS: SHOULDER WIDTH

**TYPICAL SECTION No.**

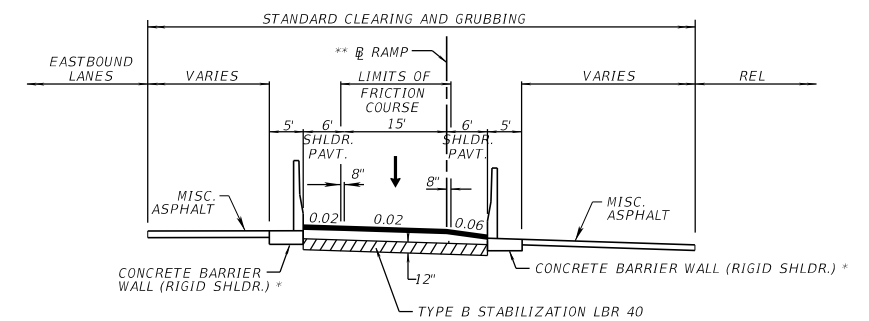


**TYPICAL SECTION SINGLE LANE SLIP RAMP NTS**

DESIGN SPEED 50 MPH

\* CONCRETE BARRIER WALL WILL BE CONSTRUCTED ON ONLY ONE OF THE TWO SHOULDERS, SEE PLAN FOR LOCATION.

CONSTRUCT RAMP PAVEMENT AND SHOULDER PAVEMENT WITH ADJACENT ROADWAY PAVEMENT DESIGN



**TYPICAL SECTION SINGLE LANE SLIP RAMP NTS**

DESIGN SPEED 50 MPH

\* CONCRETE BARRIER WALL WILL BE CONSTRUCTED ON ONLY ONE OF THE TWO SHOULDERS, SEE PLAN FOR LOCATION.

\*\* RAMP FOR R SLIP175 IS LOCATED ON THE INSIDE EDGE OF TRAVEL

CONSTRUCT RAMP PAVEMENT AND SHOULDER PAVEMENT WITH ADJACENT ROADWAY PAVEMENT DESIGN

FINANCIAL PROJECT ID	SHEET NO.
	1-27

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL            ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL            (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN    ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES. ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE            ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.    ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL   ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

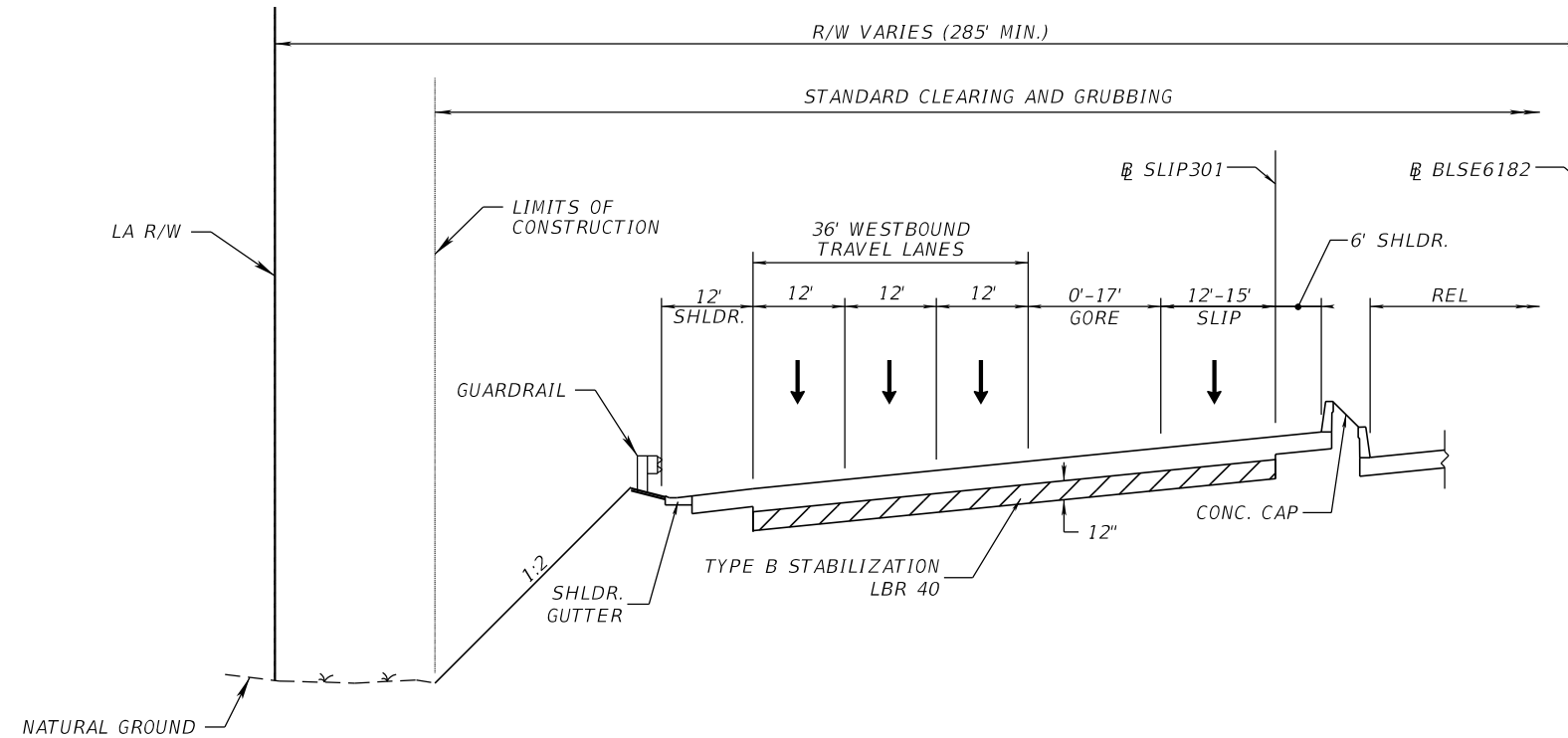
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



TYPICAL SECTION  
NEW CONSTRUCTION AT APPROACHES FOR  
WESTBOUND BRIDGE OVER US 301  
NTS  
DESIGN SPEED 60

**SELMON EXPRESSWAY WESTBOUND**

CURRENT YEAR = 2016 AADT = 35,300  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 60,700  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

FINANCIAL PROJECT ID	SHEET NO.
	1-28

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL              ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

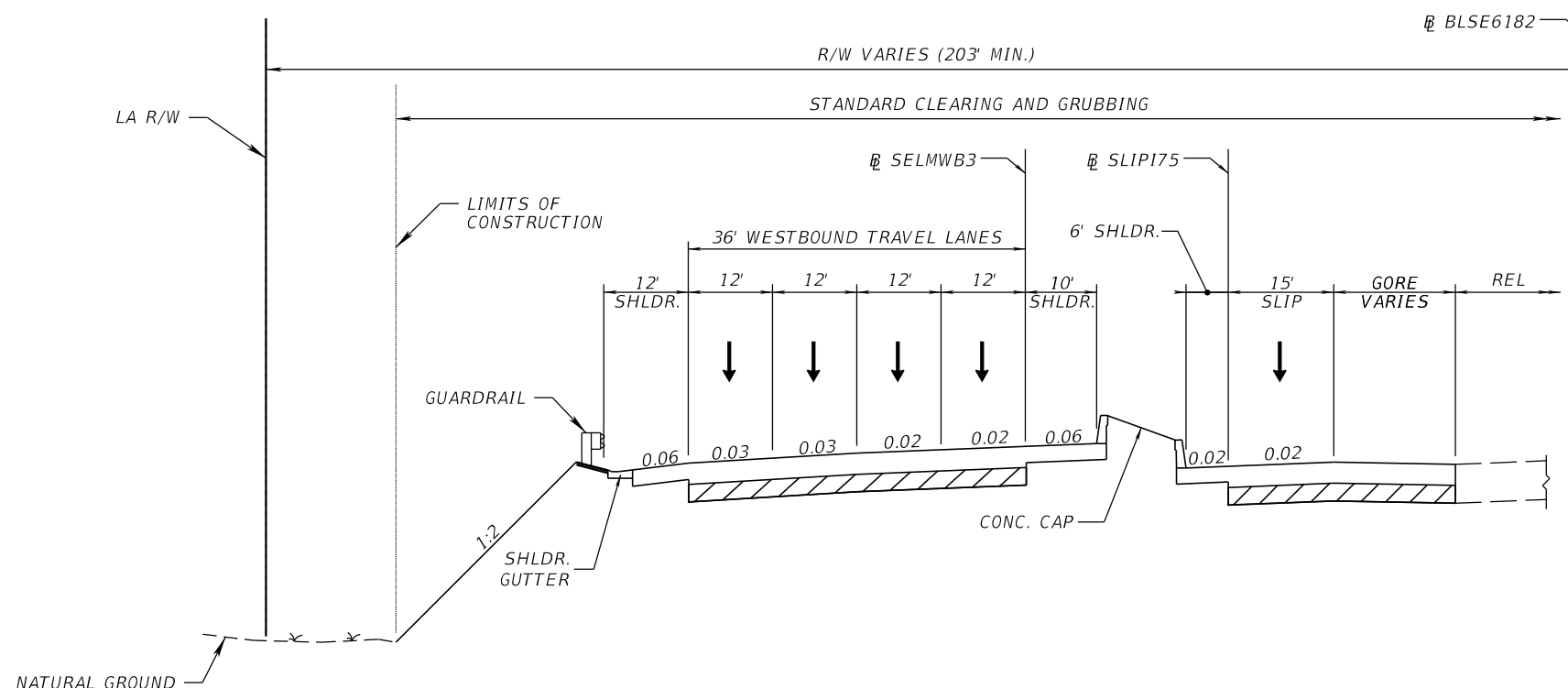
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



**TYPICAL SECTION**  
**NEW CONSTRUCTION AT APPROACHES FOR WESTBOUND**  
**BRIDGE OVER FALKENBURG ROAD**  
**NTS**  
 DESIGN SPEED 60

**SELMON EXPRESSWAY WESTBOUND**

CURRENT YEAR = 2016 AADT = 35,300  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 60,700  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

FINANCIAL PROJECT ID	SHEET NO.
	1-29

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

# PHASE 2

REVISIONS				WSP USA Inc. 2202 North West Shore Blvd., Suite 300 Tampa, Florida 33607 (813) 520-4444 Cert. of Auth. No. 1462	TAMPA HILLSBOROUGH EXPRESSWAY AUTHORITY			SHEET NO.
DATE	DESCRIPTION	DATE	DESCRIPTION		ROAD NO.	COUNTY	FINANCIAL PROJECT ID	
						618	HILLSBOROUGH	
							2	

**PROJECT CONTROLS**

**CONTEXT CLASSIFICATION**

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN              ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.         ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

**FUNCTIONAL CLASSIFICATION**

- ( ) INTERSTATE                      ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL            ( ) LOCAL
- ( ) MINOR ARTERIAL

**HIGHWAY SYSTEM**

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

**ACCESS CLASSIFICATION**

- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

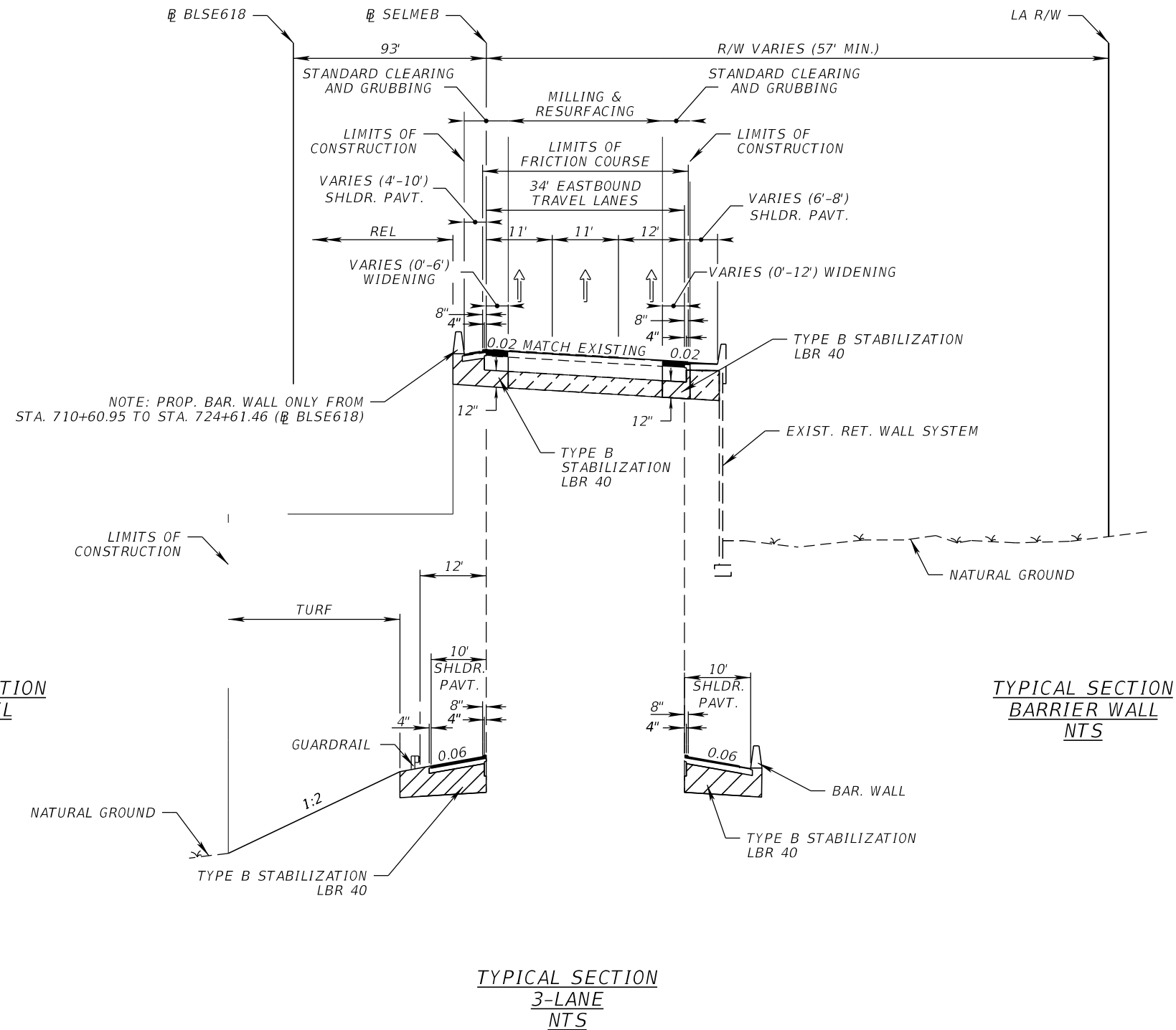
**CRITERIA**

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

**POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:**

VARIATIONS: SHOULDER WIDTH  
 EXCEPTIONS: SHOULDER WIDTH, LANE WIDTH, STOPPING SIGHT DISTANCE

**TYPICAL SECTION No.**



**TYPICAL SECTION GUARDRAIL NTS**

**TYPICAL SECTION BARRIER WALL NTS**

**TYPICAL SECTION 3-LANE NTS**

**SELMON EXPRESSWAY EASTBOUND**

CURRENT YEAR = 2016 AADT = 32,500  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 64,500  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

DESIGN SPEED 60

EXCLUDING BRIDGES NO. 100801, 100803, 100805

FINANCIAL PROJECT ID	SHEET NO.
	2-1

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

**CONTEXT CLASSIFICATION**

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

**FUNCTIONAL CLASSIFICATION**

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL            ( ) LOCAL
- ( ) MINOR ARTERIAL

**HIGHWAY SYSTEM**

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

**ACCESS CLASSIFICATION**

- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

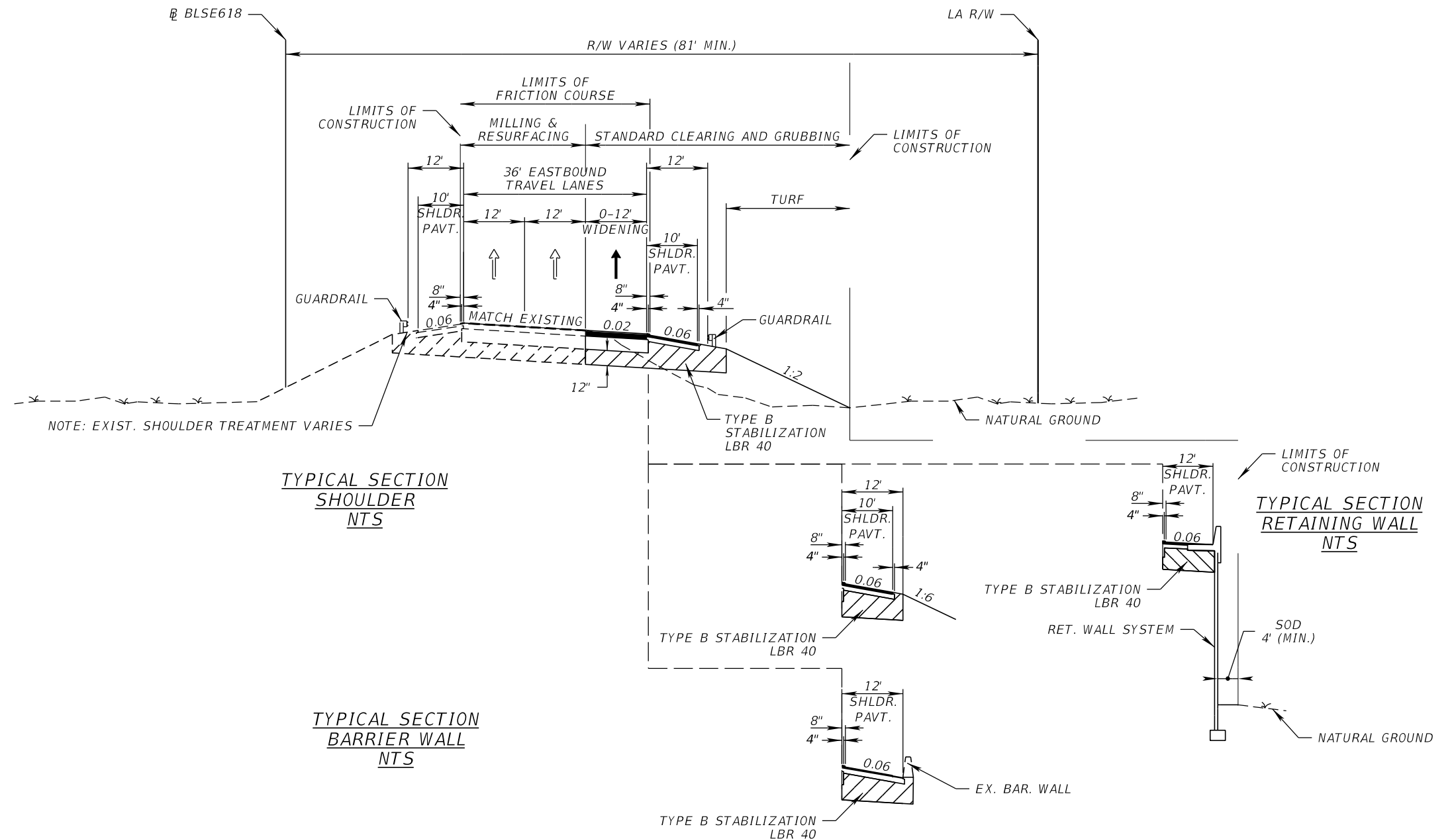
**CRITERIA**

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

**POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:**

VARIATIONS: SHOULDER WIDTH

**TYPICAL SECTION No.**



**SELMON EXPRESSWAY EASTBOUND**

CURRENT YEAR = 2016 AADT = 32,500  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 64,500  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

**TYPICAL SECTION 3-LANE NTS**  
 DESIGN SPEED 60

FINANCIAL PROJECT ID	SHEET NO.
	2-2

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL            ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

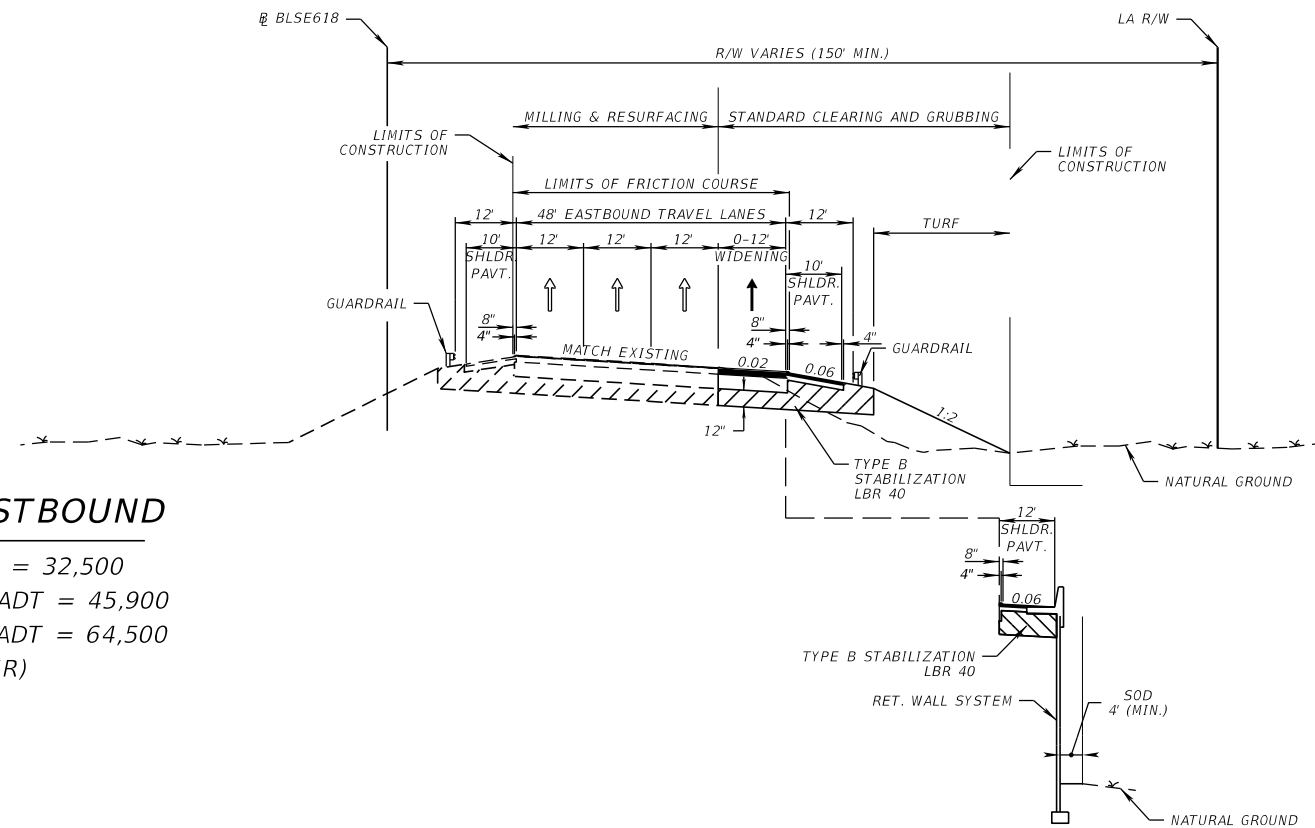
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



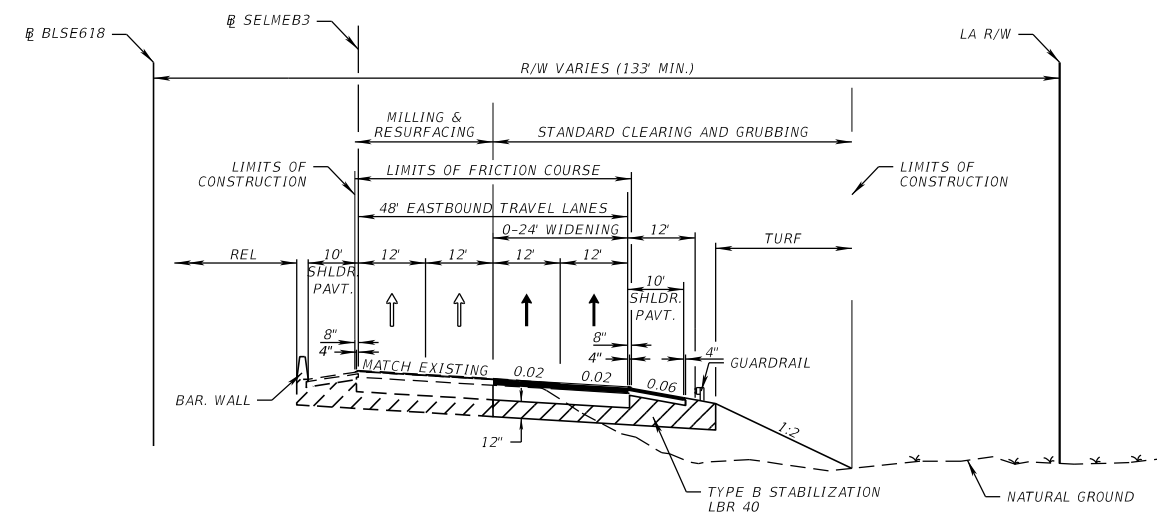
TYPICAL SECTION  
RETAINING WALL  
NTS

TYPICAL SECTION  
4-LANE  
NTS  
DESIGN SPEED 60

EXCLUDING BRIDGES NO. 100456, 100462

**SELMON EXPRESSWAY EASTBOUND**

CURRENT YEAR = 2016 AADT = 32,500  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 64,500  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%



TYPICAL SECTION  
4-LANE  
NTS  
DESIGN SPEED 60

**SELMON EXPRESSWAY EASTBOUND**

CURRENT YEAR = 2016 AADT = 32,500  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 64,500  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

FINANCIAL PROJECT ID	SHEET NO.
	2-3

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL            ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL            (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN    ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES. ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE            ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.    ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

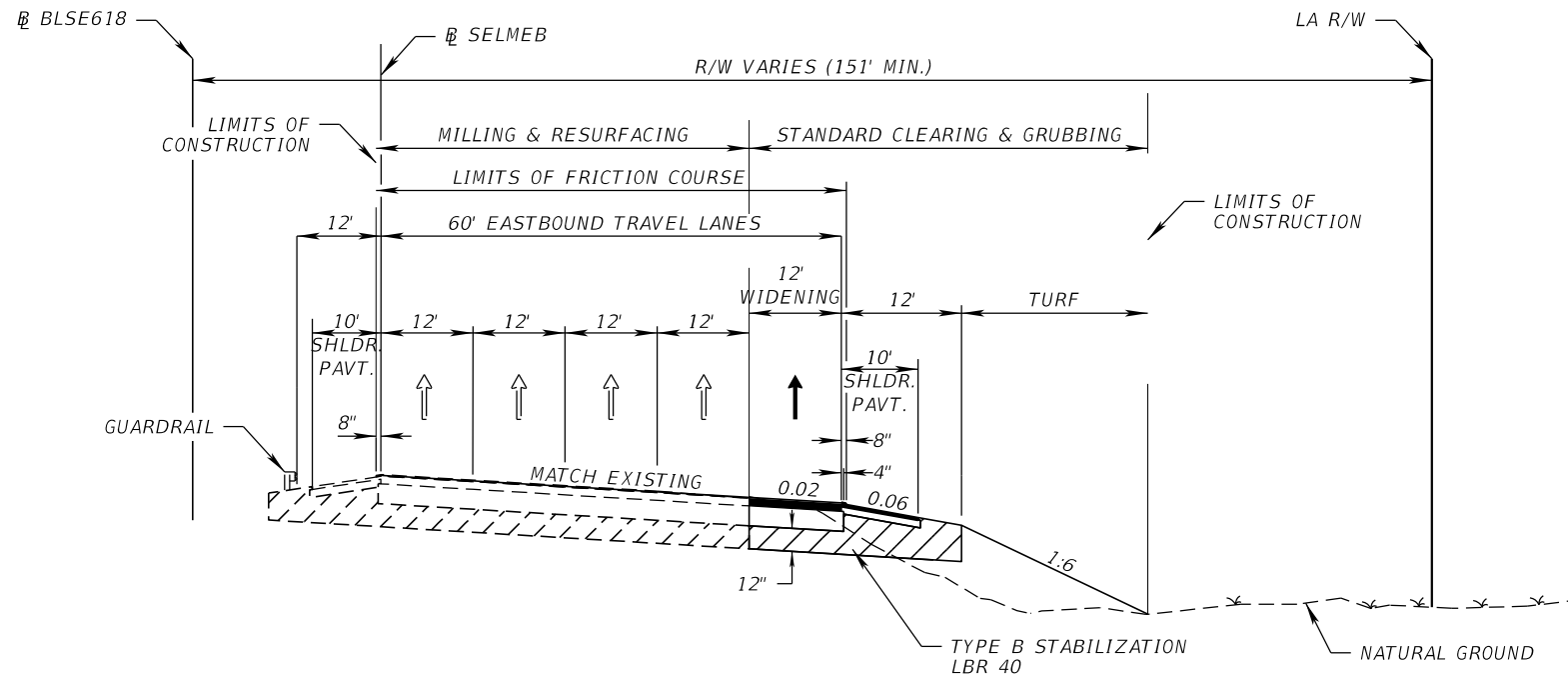
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



**TYPICAL SECTION**  
**5-LANE**  
**NTS**  
 DESIGN SPEED 60

**SELMON EXPRESSWAY EASTBOUND**

CURRENT YEAR = 2016 AADT = 32,500  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 64,500  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

FINANCIAL PROJECT ID	SHEET NO.
	2-4

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL            ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

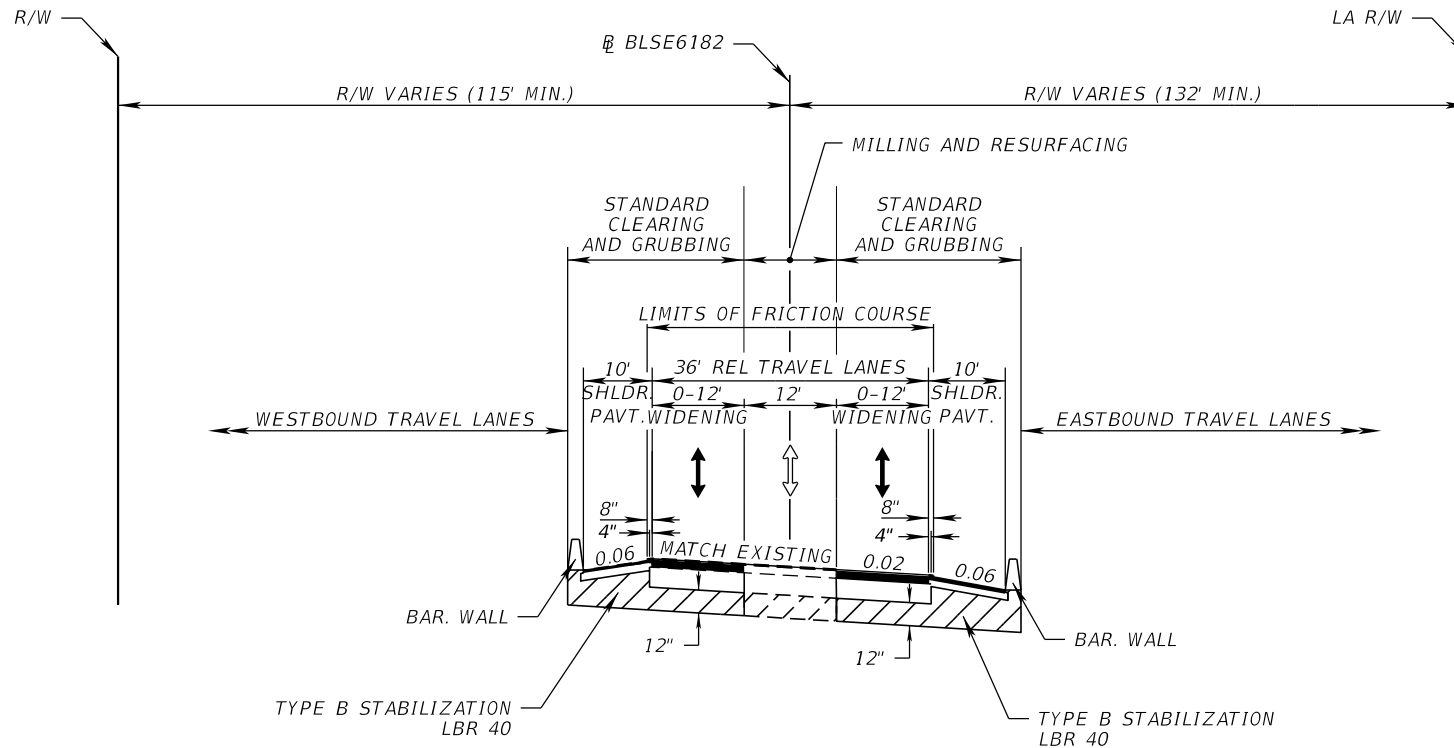
CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

VARIATIONS: SHOULDER WIDTH, MINIMUM VERTICAL CLEARANCE  
CURVE LENGTH

**TYPICAL SECTION No.**



TYPICAL SECTION  
REL  
NTS

DESIGN SPEED 60

**SELMON EXPRESSWAY WESTBOUND**

CURRENT YEAR = 2016 AADT = 35,300  
ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
ESTIMATED DESIGN YEAR = 2040 AADT = 60,700  
K = 10% D = 65% T = 3% (24 HOUR)  
DESIGN HOUR T = 3%

**SELMON EXPRESSWAY EASTBOUND**

CURRENT YEAR = 2016 AADT = 32,500  
ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
ESTIMATED DESIGN YEAR = 2040 AADT = 64,500  
K = 10% D = 65% T = 3% (24 HOUR)  
DESIGN HOUR T = 3%

EXCLUDING BRIDGES NO. 100461, 100810, 100466

FINANCIAL PROJECT ID	SHEET NO.
	2-5

**PROJECT CONTROLS**

**CONTEXT CLASSIFICATION**

- ( ) C1 : NATURAL            ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL            (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN    ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES. ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

**FUNCTIONAL CLASSIFICATION**

- ( ) INTERSTATE            ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.    ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL ( ) LOCAL
- ( ) MINOR ARTERIAL

**HIGHWAY SYSTEM**

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

**ACCESS CLASSIFICATION**

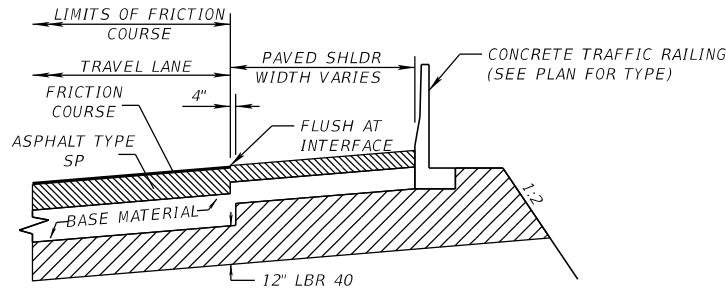
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

**CRITERIA**

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

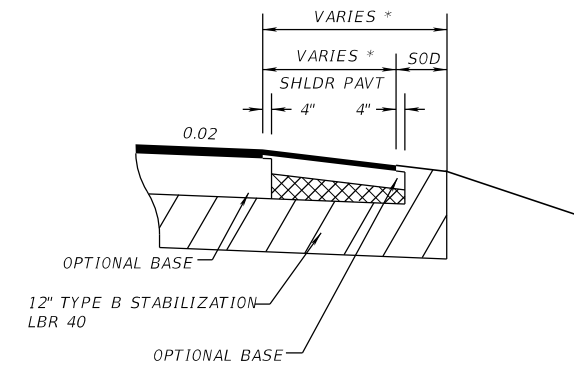
**POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:**

**TYPICAL SECTION No.**

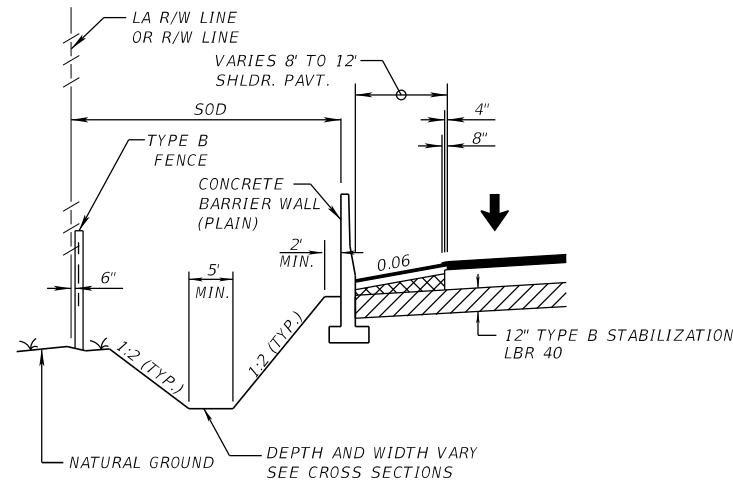


PROVIDE A 25' LONG VERTICAL TRANSITION OF THE SHOULDER PAVEMENT STRUCTURAL SECTION FROM 0.75" BELOW THE TOP OF THE HIGH SIDE FRICTION COURSE, TO FLUSH WITH THE TOP OF THE FRICTION COURSE. END THE VERTICAL SHOULDER PAVEMENT TRANSITION WHEN THE HIGH SIDE SHOULDER PAVEMENT CROSS SLOPE EQUALS 0% AND KEEP IT FLUSH WITH THE FRICTION COURSE THROUGH THE FULL SUPER DISTANCE.

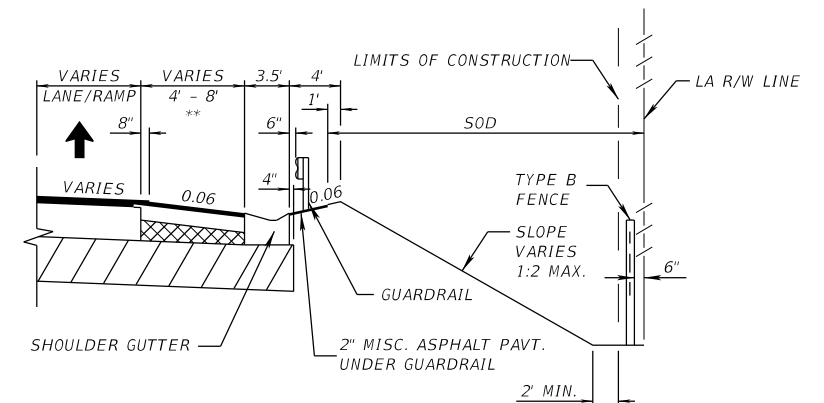
**SHOULDER PAVING ON HIGH SIDE OF SUPER ELEVATED ROADWAYS**  
NTS



**SHOULDER PAVEMENT DETAIL**  
NTS  
\*SEE PLAN FOR SPECIFIC WIDTHS



**SHOULDER WITH BARRIER WALL (RIGID SHOULDER)**  
MAINLINE AND RAMPS  
NTS



\*\* SHOULDER PAVEMENT LESS THAN 6" IN WIDTH THAT ADJOINS SHOULDER GUTTER, SHALL BE THE SAME TYPE, DEPTH AND SLOPE AS THE RAMP PAVEMENT.

**SHOULDER WITH SHOULDER GUTTER**  
MAINLINE AND RAMPS  
NTS

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

FINANCIAL PROJECT ID	SHEET NO.
	2-6

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL              ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

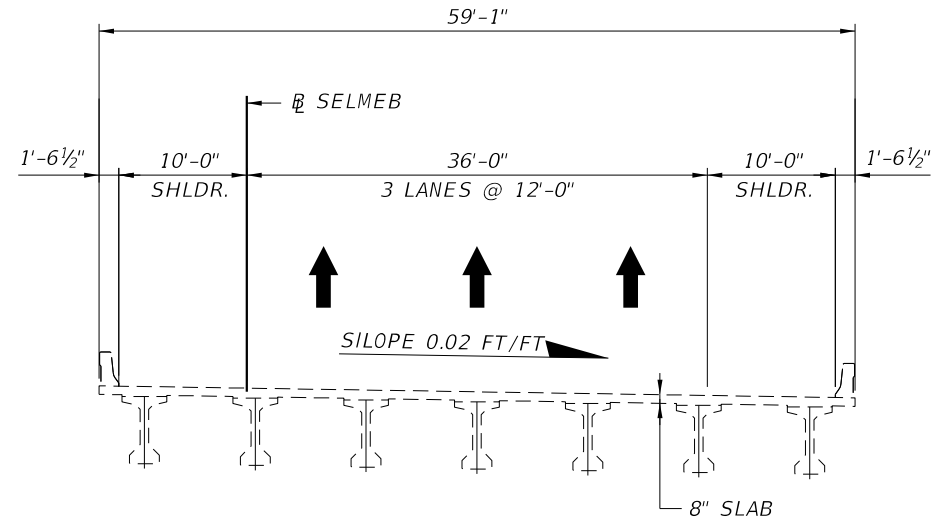
CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

VARIATIONS: VERTICAL CLEARANCE

**TYPICAL SECTION No.**



**BRIDGE NO. 100801**  
**SELMON EXPRESSWAY OVER CSX RAILROAD**  
**NTS**

**SELMON EXPRESSWAY EASTBOUND**

CURRENT YEAR = 2016 AADT = 32,500  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 64,500  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

CONCRETE REMOVAL

FINANCIAL PROJECT ID	SHEET NO.
	2-7

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN            ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.        ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                      ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.            ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL        ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

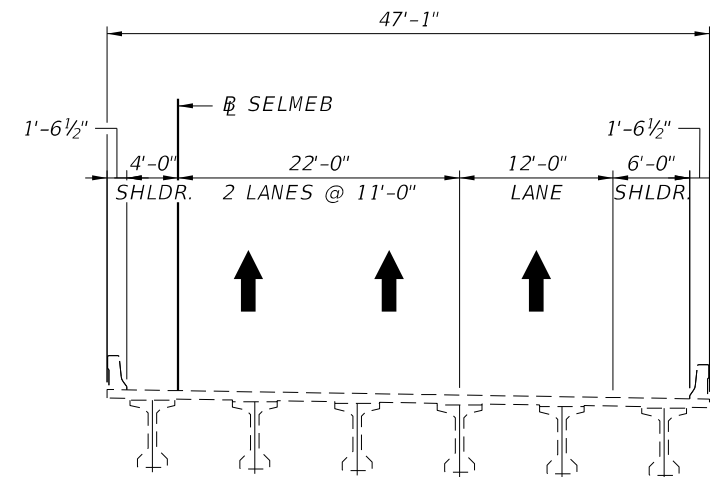
CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

VARIATIONS: LANE WIDTH

**TYPICAL SECTION No.**



**BRIDGE NO. 100803**  
**SELMON EXPRESSWAY OVER 34TH STREET**  
**NTS**

**SELMON EXPRESSWAY EASTBOUND**

CURRENT YEAR = 2016 AADT = 32,500  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 64,500  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

CONCRETE REMOVAL

FINANCIAL PROJECT ID	SHEET NO.
	2-8

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL            ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL            (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN    ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES. ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE            ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.    ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

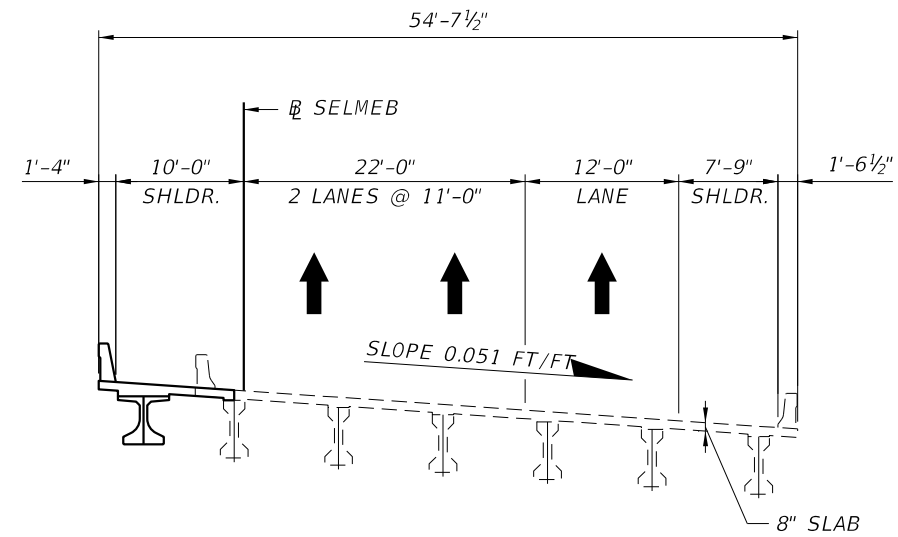
- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

VARIATIONS: LANE WIDTH  
EXCEPTIONS: STOPPING SIGHT DISTANCE

**TYPICAL SECTION No.**

BLSE618



**BRIDGE NO. 100805**  
**SELMON EXPRESSWAY OVER 39TH STREET**  
**NTS**

**SELMON EXPRESSWAY EASTBOUND**

CURRENT YEAR = 2016 AADT = 32,500  
ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
ESTIMATED DESIGN YEAR = 2040 AADT = 64,500  
K = 10% D = 65% T = 3% (24 HOUR)  
DESIGN HOUR T = 3%

CONCRETE REMOVAL

FINANCIAL PROJECT ID	SHEET NO.
	2-9

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

**CONTEXT CLASSIFICATION**

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

**FUNCTIONAL CLASSIFICATION**

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL              ( ) LOCAL
- ( ) MINOR ARTERIAL

**HIGHWAY SYSTEM**

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

**ACCESS CLASSIFICATION**

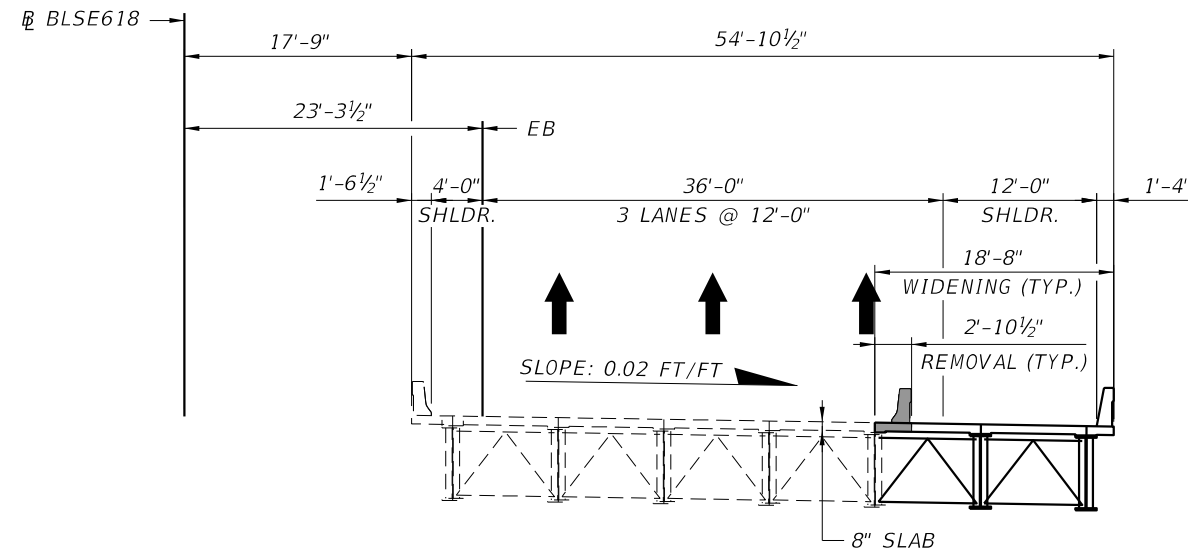
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

**CRITERIA**

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

**POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:**

**TYPICAL SECTION No.**



**BRIDGE NO. 100454**  
**SELMON EXPRESSWAY OVER 50TH STREET**  
**NTS**

**SELMON EXPRESSWAY EASTBOUND**

CURRENT YEAR = 2016 AADT = 32,500  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 64,500  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

CONCRETE REMOVAL

FINANCIAL PROJECT ID	SHEET NO.
	2-10

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL              ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

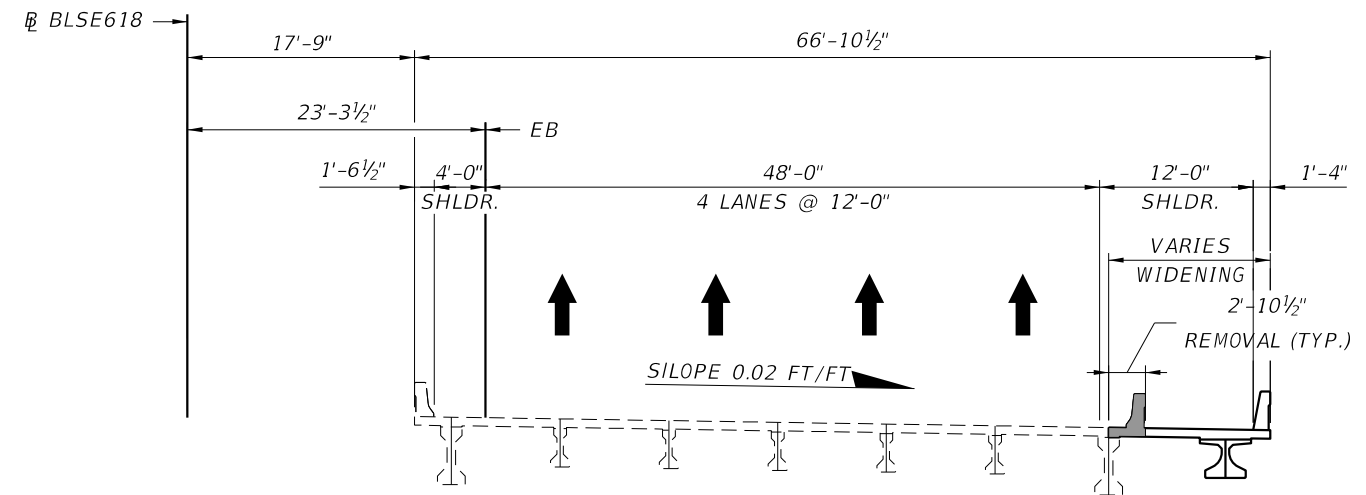
CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

EXCEPTIONS: VERTICAL CLEARANCE

**TYPICAL SECTION No.**



**BRIDGE NO. 100456**  
**SELMON EXPRESSWAY OVER SCL RR**  
**NTS**

**SELMON EXPRESSWAY EASTBOUND**

CURRENT YEAR = 2016 AADT = 32,500  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 64,500  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

CONCRETE REMOVAL

FINANCIAL PROJECT ID	SHEET NO.
	2-11

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL            ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

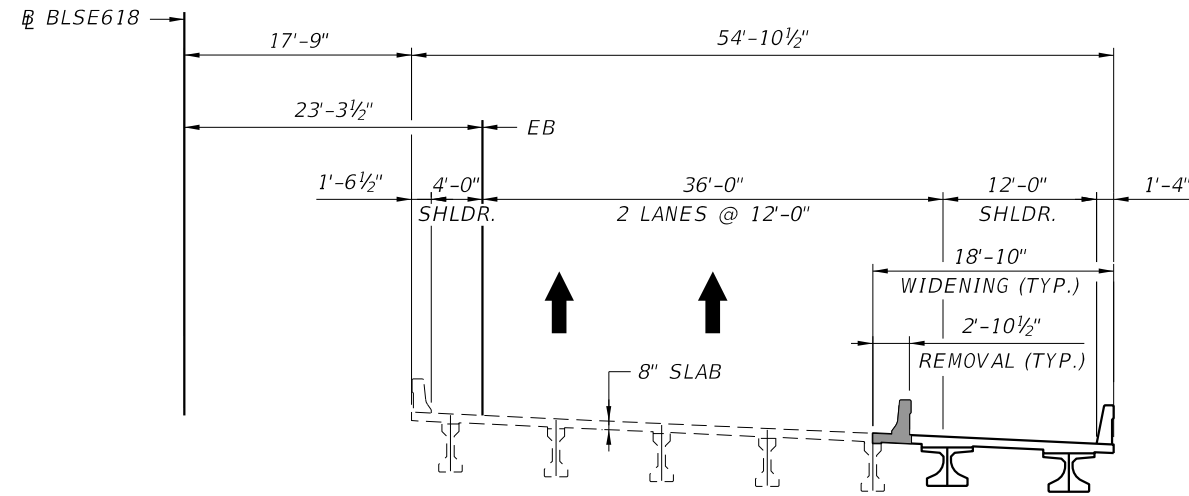
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



**BRIDGE NO. 100458**  
**SELMON EXPRESSWAY OVER MAYDELL DRIVE**  
**NTS**

**SELMON EXPRESSWAY EASTBOUND**

CURRENT YEAR = 2016 AADT = 32,500  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 64,500  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

CONCRETE REMOVAL

FINANCIAL PROJECT ID	SHEET NO.
	2-12

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN              ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.        ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                      ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.              ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL          ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

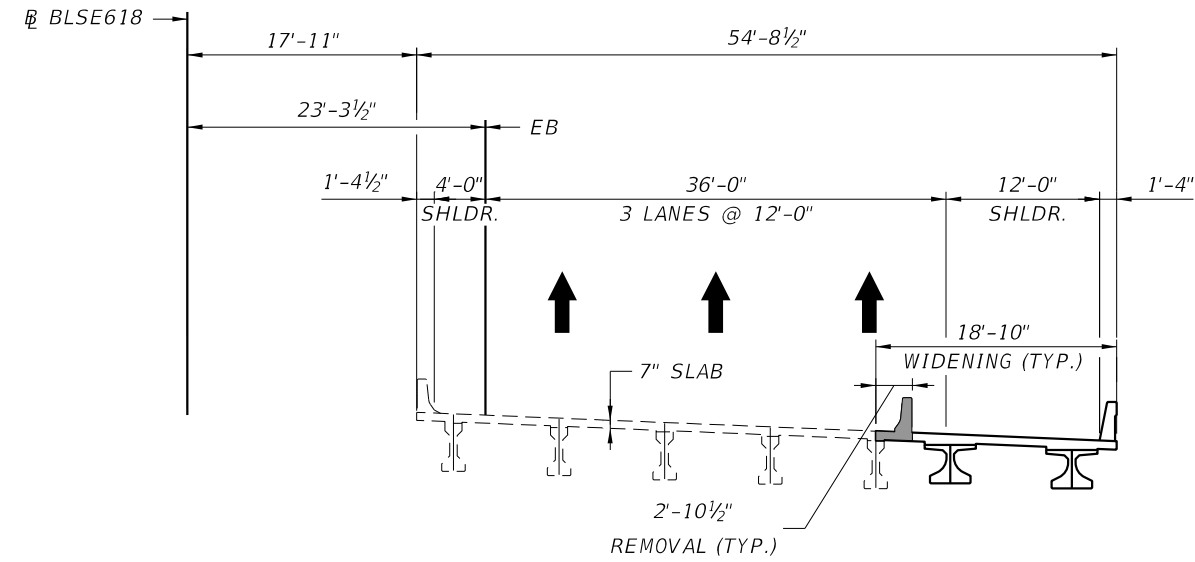
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



**BRIDGE NO. 100460**  
**SELMON EXPRESSWAY OVER BYPASS CANAL (PALM RIVER)**  
**NTS**

**SELMON EXPRESSWAY EASTBOUND**

CURRENT YEAR = 2016 AADT = 32,500  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 64,500  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

CONCRETE REMOVAL

FINANCIAL PROJECT ID	SHEET NO.
	2-13

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL              ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

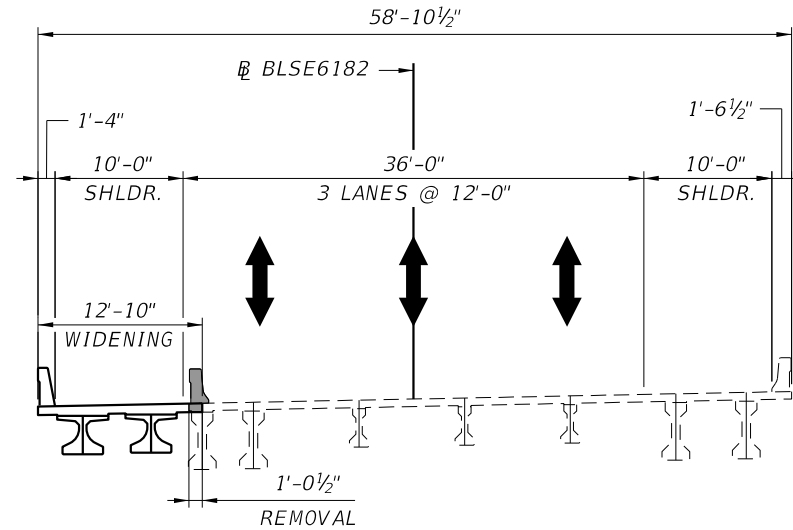
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



REL  
BRIDGE NO. 100461  
SELMON EXPRESSWAY OVER PALM RIVER ROAD  
NTS

**SELMON EXPRESSWAY EASTBOUND**

CURRENT YEAR = 2016 AADT = 32,500  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 64,500  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

CONCRETE REMOVAL

FINANCIAL PROJECT ID	SHEET NO.
	2-14

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL              ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

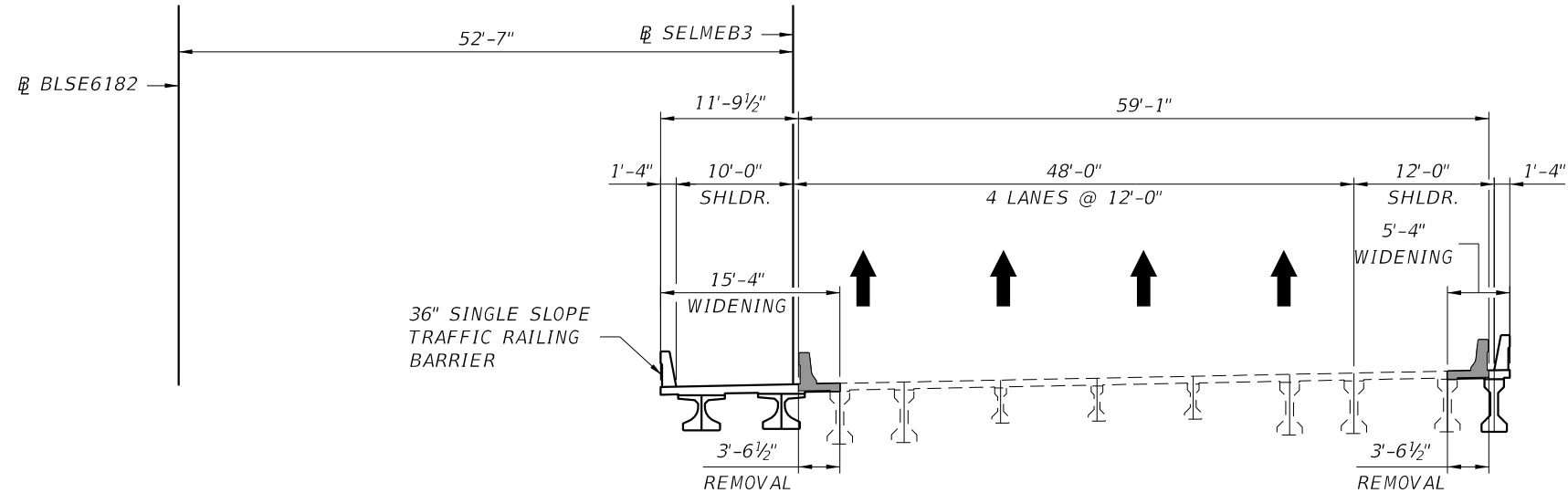
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



**BRIDGE NO. 100462**  
**SELMON EXPRESSWAY OVER PALM RIVER ROAD**  
**NTS**

**SELMON EXPRESSWAY EASTBOUND**

CURRENT YEAR = 2016 AADT = 32,500  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 64,500  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

CONCRETE REMOVAL

FINANCIAL PROJECT ID	SHEET NO.
	2-15

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL            ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

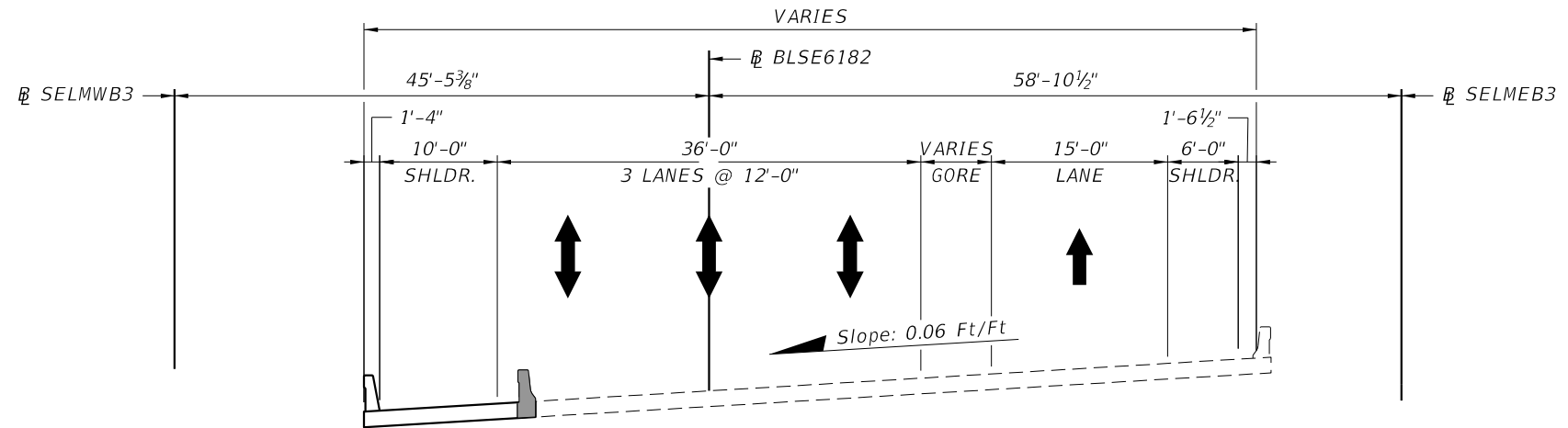
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



REL  
BRIDGE NO. 100810  
SELMON EXPRESSWAY OVER DELANEY CREEK  
NTS

**SELMON EXPRESSWAY WESTBOUND**

CURRENT YEAR = 2016 AADT = 35,300  
ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
ESTIMATED DESIGN YEAR = 2040 AADT = 60,700  
K = 10% D = 65% T = 3% (24 HOUR)  
DESIGN HOUR T = 3%

**SELMON EXPRESSWAY EASTBOUND**

CURRENT YEAR = 2016 AADT = 32,500  
ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
ESTIMATED DESIGN YEAR = 2040 AADT = 64,500  
K = 10% D = 65% T = 3% (24 HOUR)  
DESIGN HOUR T = 3%

CONCRETE REMOVAL

FINANCIAL PROJECT ID	SHEET NO.
	2-16

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL            ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL            (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN    ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES. ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE            ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.    ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

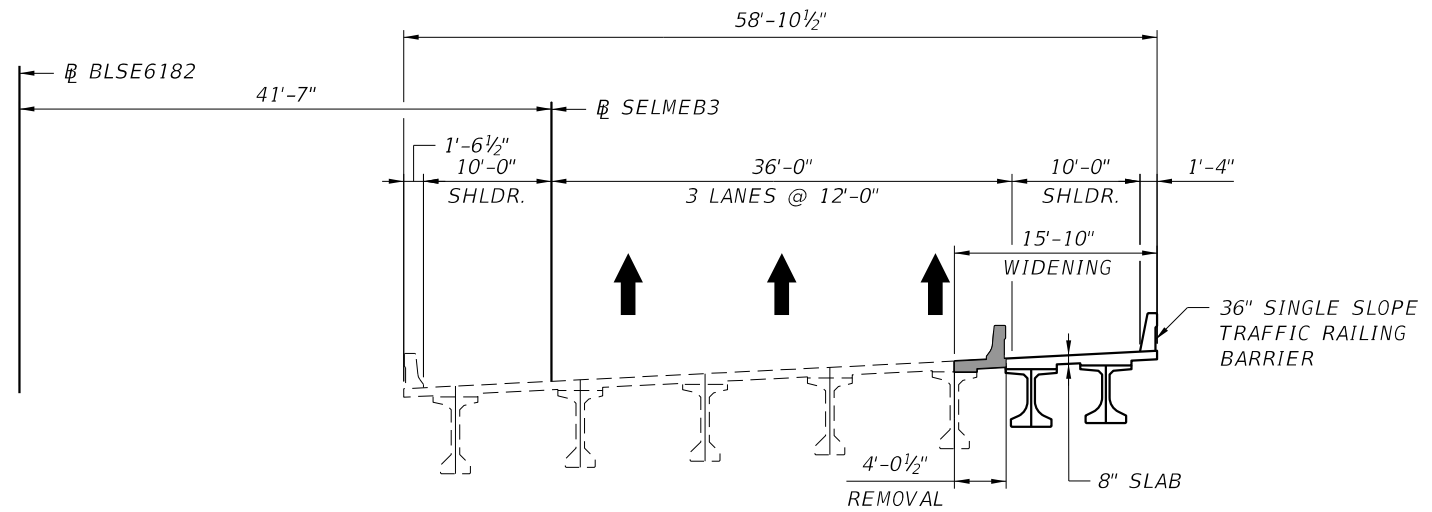
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



**BRIDGE NO. 100811**  
**SELMON EXPRESSWAY OVER US301**  
**NTS**

**SELMON EXPRESSWAY EASTBOUND**

CURRENT YEAR = 2016 AADT = 32,500  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 64,500  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

CONCRETE REMOVAL

FINANCIAL PROJECT ID	SHEET NO.
	2-17

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL            ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL            (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN    ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES. ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE            ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.    ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL   ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

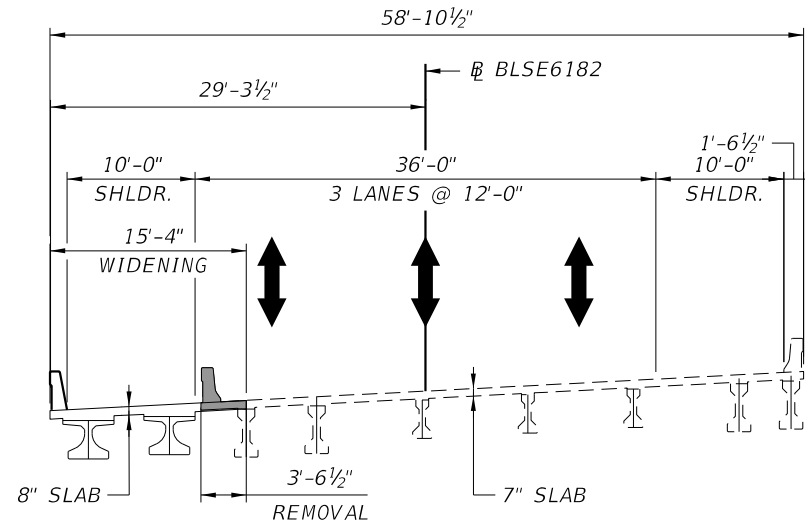
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



**BRIDGE NO. 100466**  
**SELMON EXPRESSWAY OVER US301**  
**NTS**

**SELMON EXPRESSWAY WESTBOUND**

CURRENT YEAR = 2016 AADT = 35,300  
ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
ESTIMATED DESIGN YEAR = 2040 AADT = 60,700  
K = 10% D = 65% T = 3% (24 HOUR)  
DESIGN HOUR T = 3%

**SELMON EXPRESSWAY EASTBOUND**

CURRENT YEAR = 2016 AADT = 32,500  
ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
ESTIMATED DESIGN YEAR = 2040 AADT = 64,500  
K = 10% D = 65% T = 3% (24 HOUR)  
DESIGN HOUR T = 3%

CONCRETE REMOVAL

FINANCIAL PROJECT ID	SHEET NO.
	2-18

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN              ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.          ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                      ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.              ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL          ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

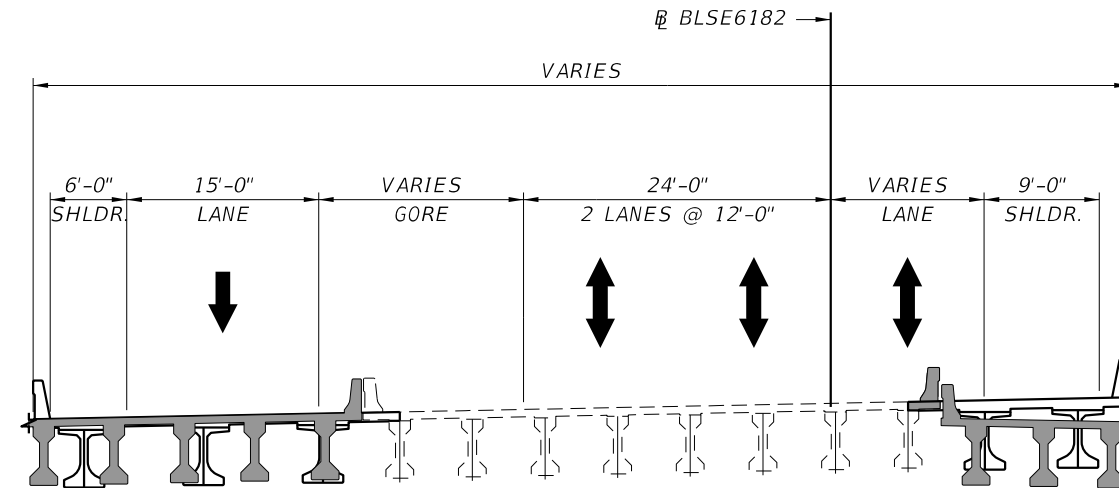
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



**BRIDGE NO. 100490**  
**SELMON EXPRESSWAY OVER FALKENBURG ROAD**  
**NTS**

**SELMON EXPRESSWAY WESTBOUND**

CURRENT YEAR = 2016 AADT = 35,300  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 60,700  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

**SELMON EXPRESSWAY EASTBOUND**

CURRENT YEAR = 2016 AADT = 32,500  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 64,500  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

CONCRETE REMOVAL

FINANCIAL PROJECT ID	SHEET NO.
	2-19

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL              ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

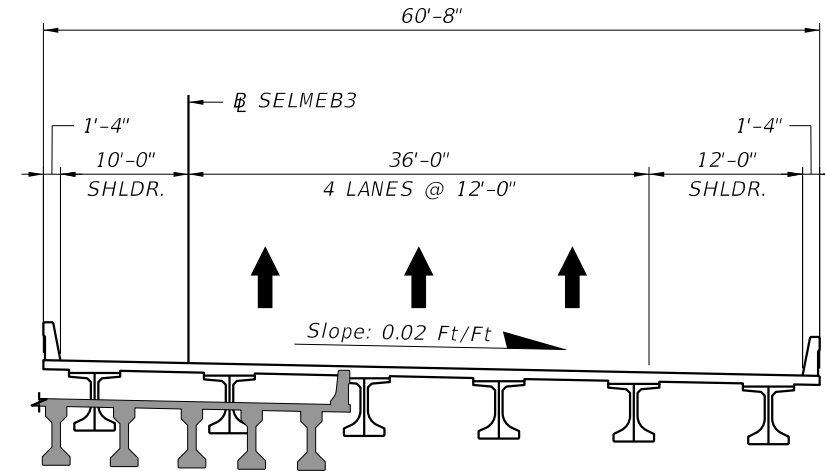
CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**

BLSE6182



**BRIDGE - NEW CONSTRUCTION**  
**SELMON EXPRESSWAY OVER FALKENBURG ROAD**  
**NTS**

**SELMON EXPRESSWAY EASTBOUND**

CURRENT YEAR = 2016 AADT = 32,500  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 64,500  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

CONCRETE REMOVAL

FINANCIAL PROJECT ID	SHEET NO.
	2-20

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL              ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

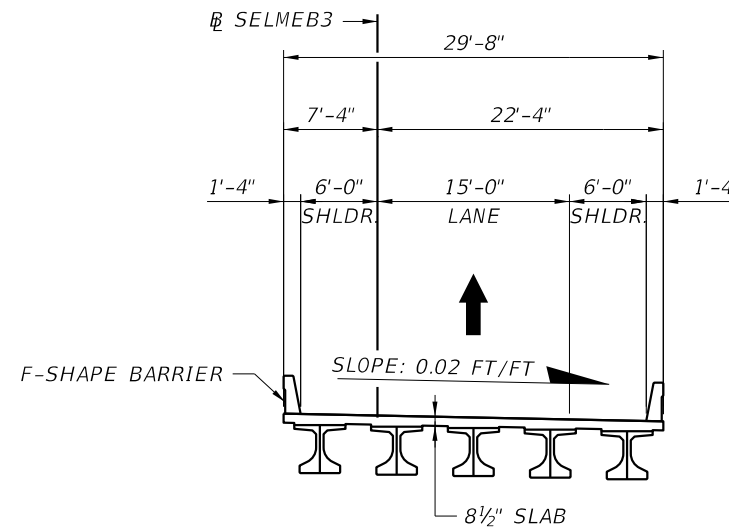
CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

VARIATIONS: MINIMUM VERTICAL CURVE LENGTH

**TYPICAL SECTION No.**



**BRIDGE - NEW CONSTRUCTION**  
**EASTBOUND EXIT RAMP TO I-75 NORTHBOUND OVER I-75 SOUTBOUND**  
**NTS**

**SELMON EXPRESSWAY EASTBOUND**

CURRENT YEAR = 2016 AADT = 32,500  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 64,500  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

CONCRETE REMOVAL

FINANCIAL PROJECT ID	SHEET NO.
	2-21

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

**CONTEXT CLASSIFICATION**

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

**FUNCTIONAL CLASSIFICATION**

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL              ( ) LOCAL
- ( ) MINOR ARTERIAL

**HIGHWAY SYSTEM**

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

**ACCESS CLASSIFICATION**

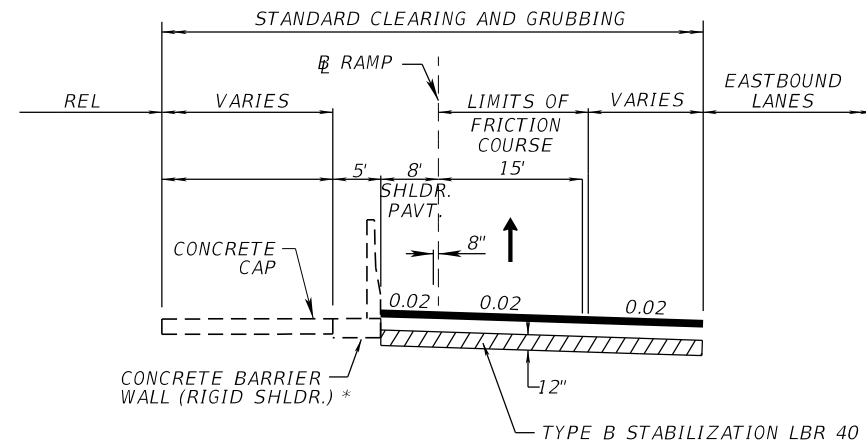
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

**CRITERIA**

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

**POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:**

**TYPICAL SECTION No.**



**TYPICAL SECTION  
SINGLE LANE SLIP RAMP  
NTS**

DESIGN SPEED 50 MPH

\* CONCRETE BARRIER WALL WILL BE CONSTRUCTED ON ONLY ONE OF THE TWO SHOULDERS, SEE PLAN FOR LOCATION. BARRIER WALL IS PROPOSED FROM STA. 123+62.73 TO STA. 126+18.31 (@ PALMSLIP)

CONSTRUCT RAMP PAVEMENT AND SHOULDER PAVEMENT WITH ADJACENT ROADWAY PAVEMENT DESIGN

FINANCIAL PROJECT ID	SHEET NO.
	2-22

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

**CONTEXT CLASSIFICATION**

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

**FUNCTIONAL CLASSIFICATION**

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL              ( ) LOCAL
- ( ) MINOR ARTERIAL

**HIGHWAY SYSTEM**

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

**ACCESS CLASSIFICATION**

- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

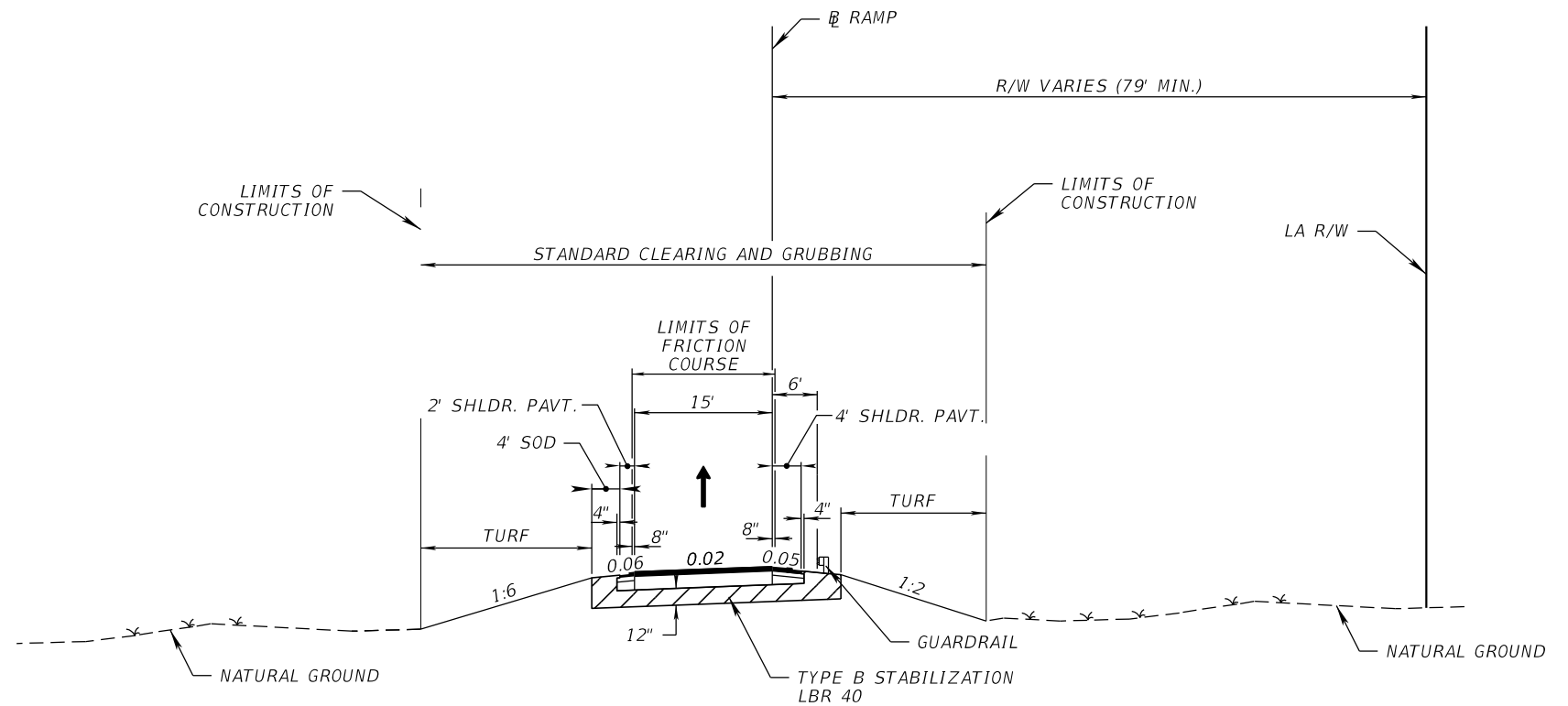
**CRITERIA**

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

**POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:**

STOPPING SIGHT DISTANCE (EXCEPTION)

**TYPICAL SECTION No.**



**TYPICAL SECTION  
1-LANE RAMP - NEW CONSTRUCTION  
NTS**

DESIGN SPEED VARIES:  
20 MPH AT TERMINALS  
30-35 MPH ON LOOP RAMPS  
40-50 MPH AT GORE LOCATIONS

FINANCIAL PROJECT ID	SHEET NO.
	2-23

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

**CONTEXT CLASSIFICATION**

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

**FUNCTIONAL CLASSIFICATION**

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL            ( ) LOCAL
- ( ) MINOR ARTERIAL

**HIGHWAY SYSTEM**

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

**ACCESS CLASSIFICATION**

- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

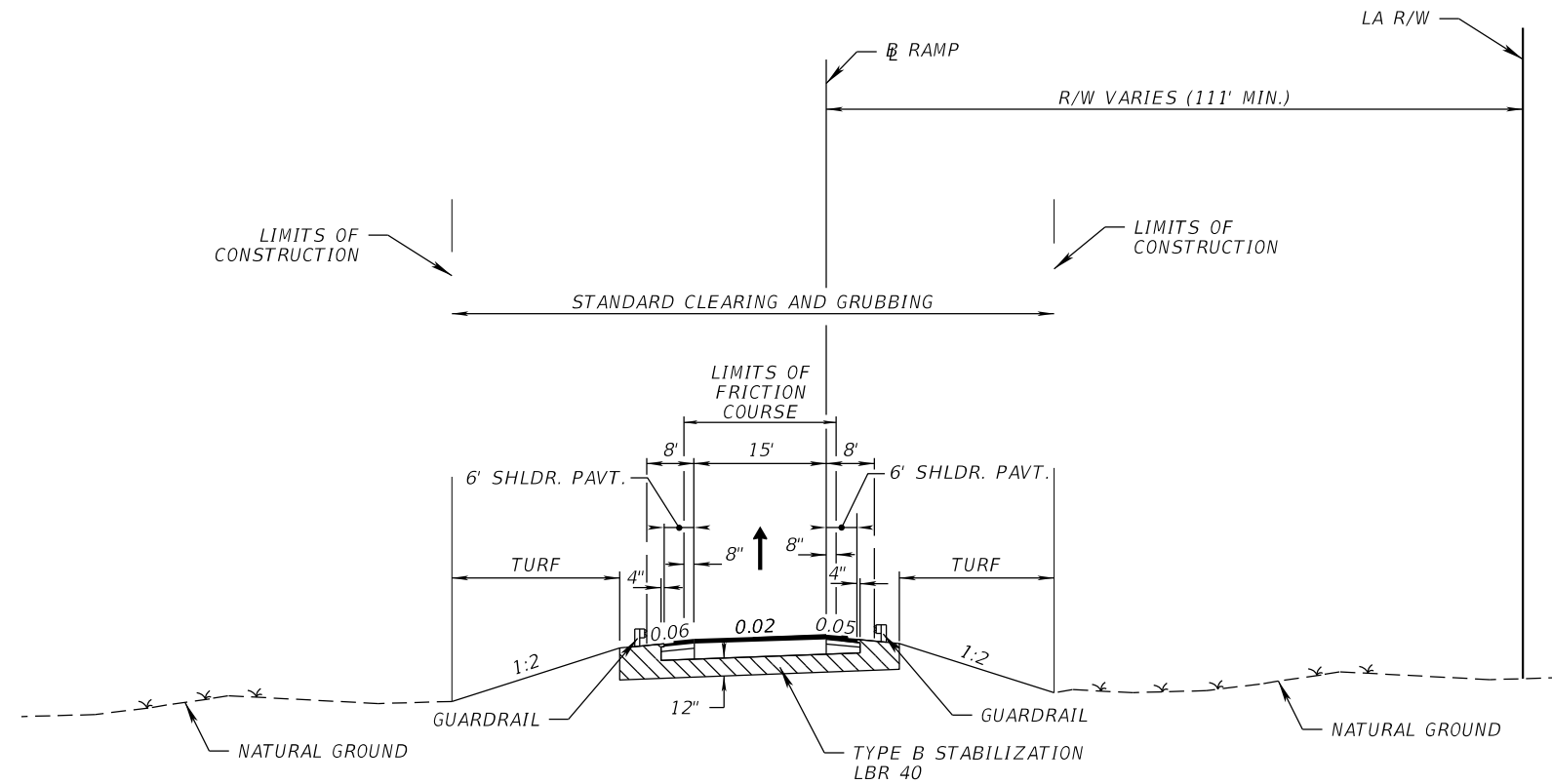
**CRITERIA**

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

**POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:**

VARIATIONS: DESIGN SPEED  
EXCEPTIONS: STOPPING SIGHT DISTANCE

**TYPICAL SECTION No.**



**TYPICAL SECTION  
1-LANE RAMP - NEW CONSTRUCTION  
NTS**

DESIGN SPEED VARIES:  
20 MPH AT TERMINALS  
30-35 MPH ON LOOP RAMPS  
40-50 MPH AT GORE LOCATIONS

FINANCIAL PROJECT ID	SHEET NO.
	2-24

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

**CONTEXT CLASSIFICATION**

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

**FUNCTIONAL CLASSIFICATION**

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL            ( ) LOCAL
- ( ) MINOR ARTERIAL

**HIGHWAY SYSTEM**

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

**ACCESS CLASSIFICATION**

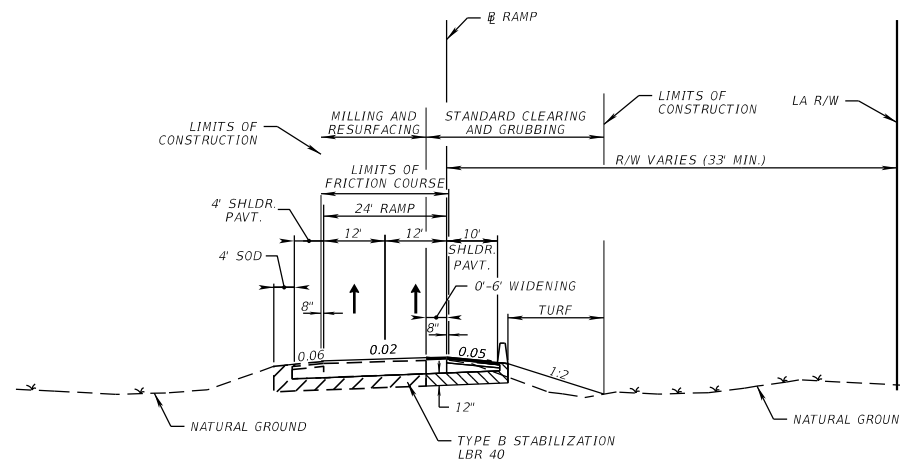
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

**CRITERIA**

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

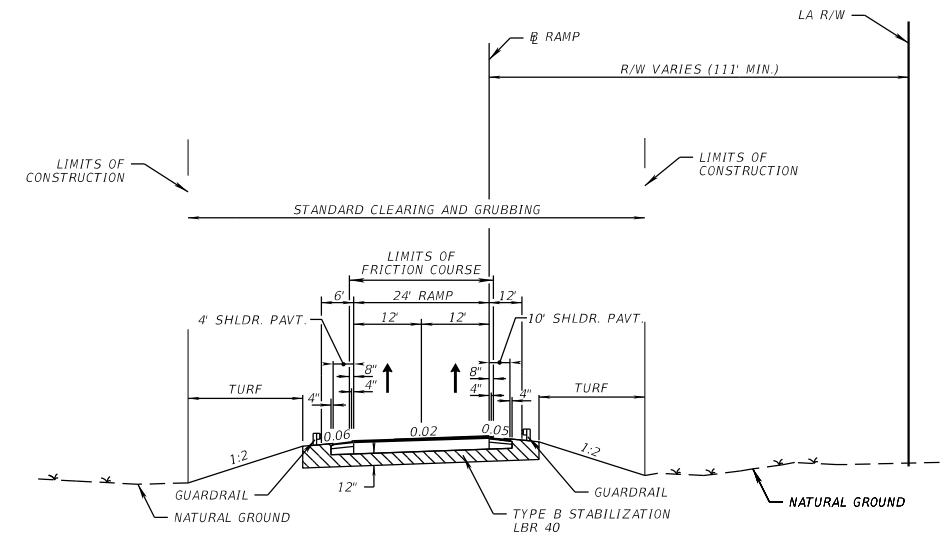
**POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:**

**TYPICAL SECTION No.**



**TYPICAL SECTION  
2-LANE RAMP - WIDENING  
NTS**

DESIGN SPEED VARIES:  
20 MPH AT TERMINALS  
30-35 MPH ON LOOP RAMPS  
40-50 MPH AT GORE LOCATIONS



**TYPICAL SECTION  
2-LANE RAMP - NEW CONSTRUCTION  
NTS**

DESIGN SPEED VARIES:  
20 MPH AT TERMINALS  
30-35 MPH ON LOOP RAMPS  
40-50 MPH AT GORE LOCATIONS

FINANCIAL PROJECT ID	SHEET NO.
	2-25

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.              ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                  ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL                ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

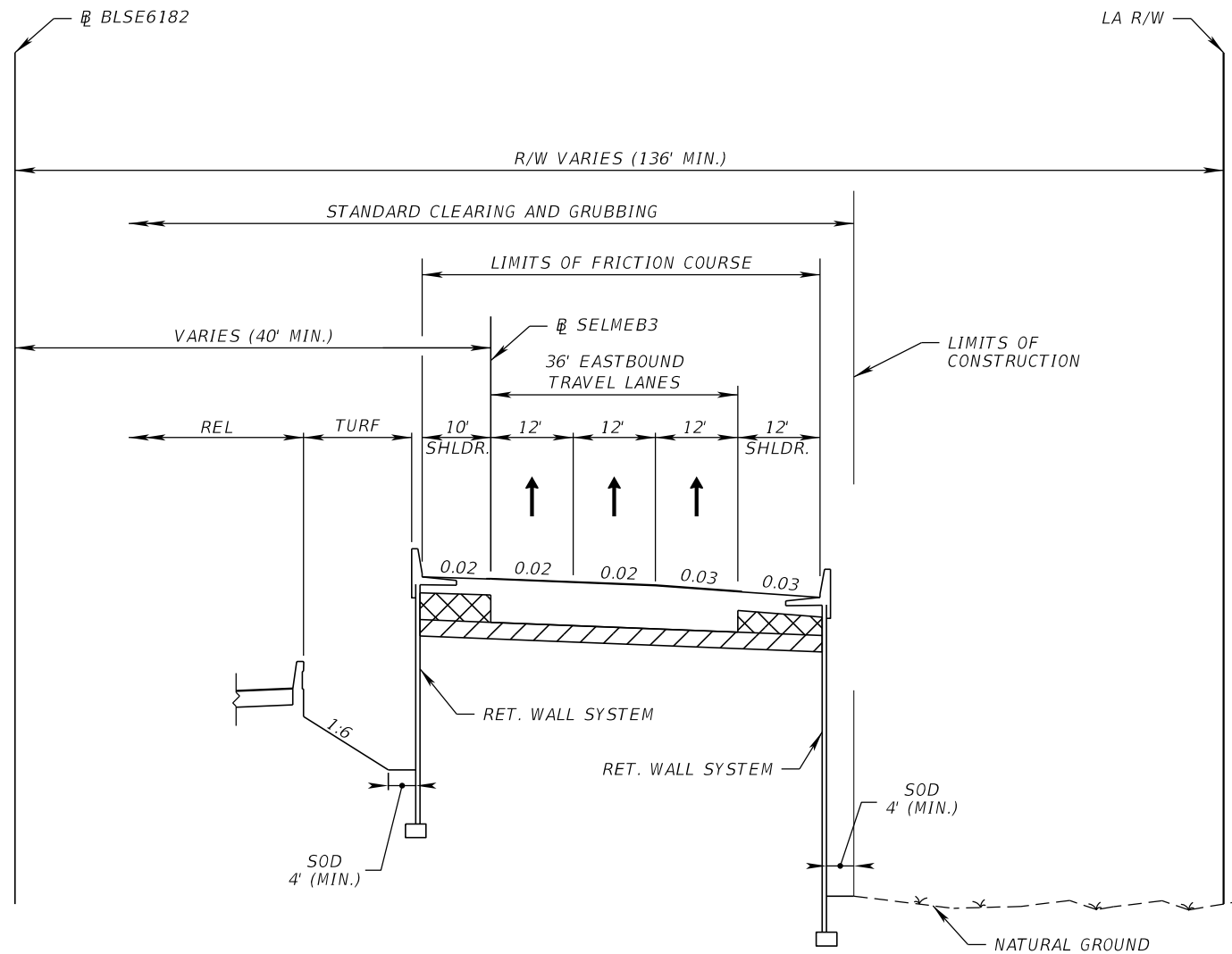
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



**TYPICAL SECTION  
NEW CONSTRUCTION AT APPROACHES FOR  
EASTBOUND BRIDGE OVER FALKENBURG ROAD  
NTS**

DESIGN SPEED 60

**SELMON EXPRESSWAY EASTBOUND**

CURRENT YEAR = 2016 AADT = 32,500  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 64,500  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

FINANCIAL PROJECT ID	SHEET NO.
	2-26

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

# PHASE 3

REVISIONS				WSP USA Inc. 2202 North West Shore Blvd., Suite 300 Tampa, Florida 33607 (813) 520-4444 Cert. of Auth. No. 1462	TAMPA HILLSBOROUGH EXPRESSWAY AUTHORITY			SHEET NO.
DATE	DESCRIPTION	DATE	DESCRIPTION		ROAD NO.	COUNTY	FINANCIAL PROJECT ID	
					618	HILLSBOROUGH		
							3	

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL            ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

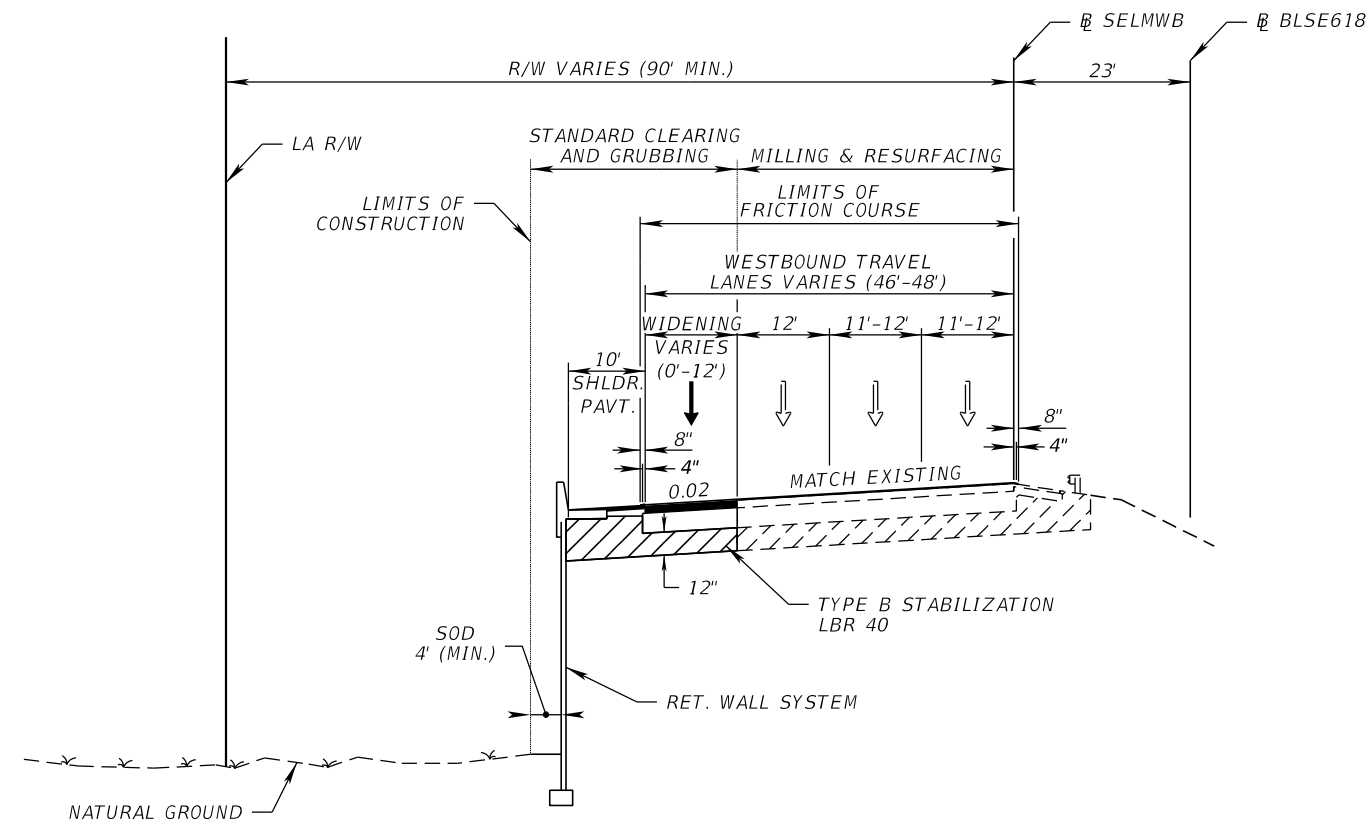
CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

VARIATIONS: SHOULDER WIDTH  
 EXCEPTIONS: SHOULDER WIDTH, LANE WIDTH, STOPPING SIGHT DISTANCE

**TYPICAL SECTION No.**



**TYPICAL SECTION**  
**4-LANE**  
**NTS**  
 DESIGN SPEED 60

EXCLUDING BRIDGES NO. 100444, 100835

**SELMON EXPRESSWAY WESTBOUND**

CURRENT YEAR = 2016 AADT = 35,300  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 60,700  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

FINANCIAL PROJECT ID	SHEET NO.
	3-1

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                 ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL              ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

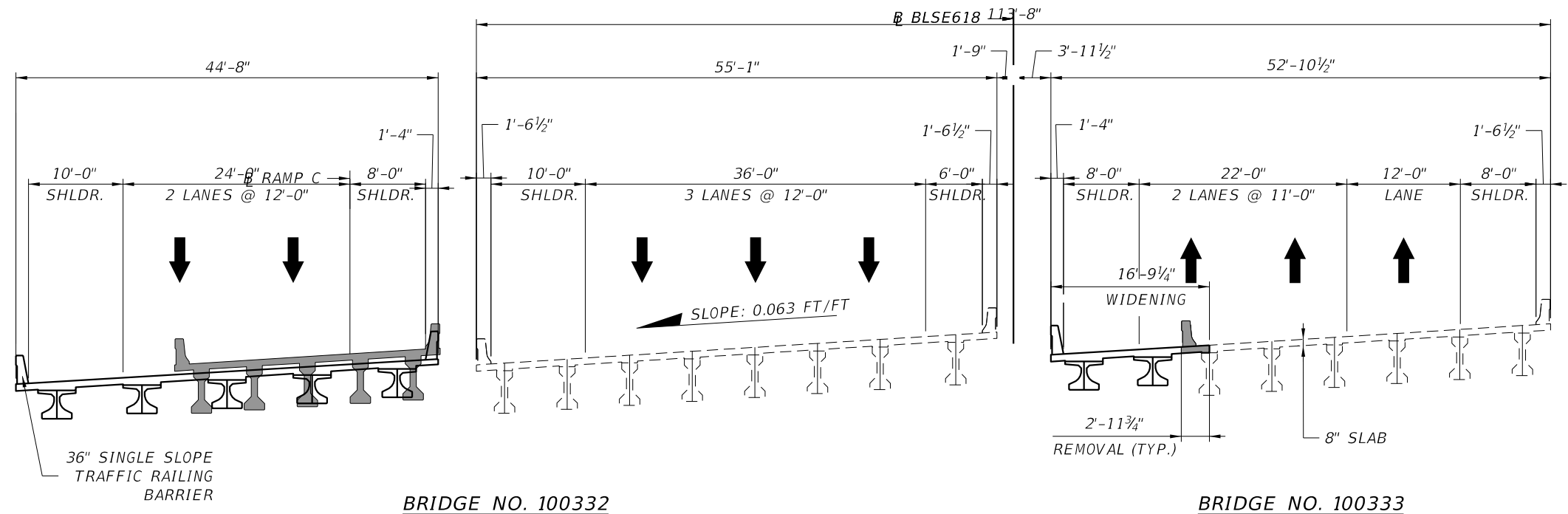
CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

VARIATIONS: SHOULDER WIDTH, VERTICAL CLEARANCE  
 EXCEPTIONS: SHOULDER WIDTH, LANE WIDTH, STOPPING SIGHT DISTANCE

**TYPICAL SECTION No.**



**BRIDGE NO. 100332**

**BRIDGE NO. 100333**

**SELMON EXPRESSWAY WESTBOUND**

CURRENT YEAR = 2016 AADT = 35,300  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 60,700  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

**SELMON EXPRESSWAY EASTBOUND**

CURRENT YEAR = 2016 AADT = 32,500  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 64,500  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

**SELMON EXPRESSWAY DOWNTOWN TAMPA VIADUCT**  
NTS

CONCRETE REMOVAL

FINANCIAL PROJECT ID	SHEET NO.
	3-2

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

**CONTEXT CLASSIFICATION**

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

**FUNCTIONAL CLASSIFICATION**

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL            ( ) LOCAL
- ( ) MINOR ARTERIAL

**HIGHWAY SYSTEM**

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

**ACCESS CLASSIFICATION**

- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

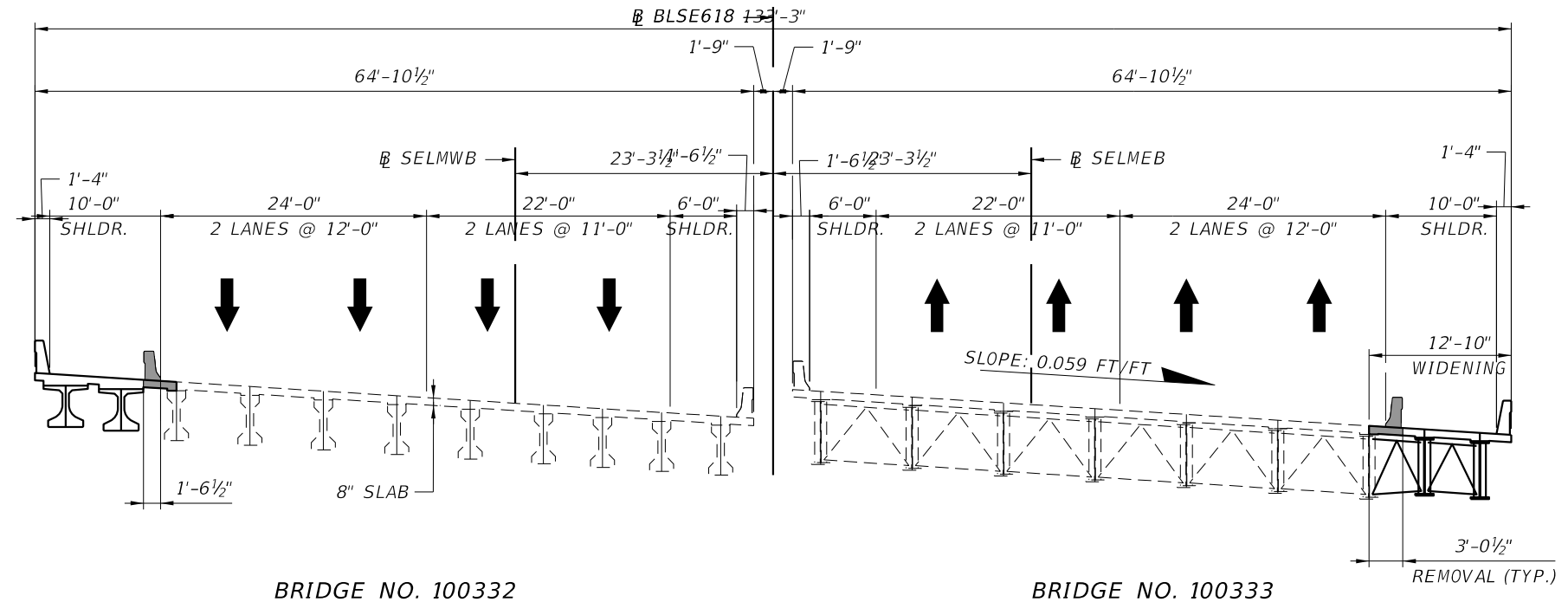
**CRITERIA**

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

**POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:**

VARIATIONS: SHOULDER WIDTH, HORIZONTAL STOPPING SIGHT DISTANCE  
 EXCEPTIONS: SHOULDER WIDTH, LANE WIDTH, STOPPING SIGHT DISTANCE

**TYPICAL SECTION No.**



**BRIDGE NO. 100332**

**BRIDGE NO. 100333**

**SELMON EXPRESSWAY WESTBOUND**

CURRENT YEAR = 2016 AADT = 35,300  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 60,700  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

**SELMON EXPRESSWAY EASTBOUND**

CURRENT YEAR = 2016 AADT = 32,500  
 ESTIMATED OPENING YEAR = 2026 AADT = 45,900  
 ESTIMATED DESIGN YEAR = 2040 AADT = 64,500  
 K = 10% D = 65% T = 3% (24 HOUR)  
 DESIGN HOUR T = 3%

**SELMON EXPRESSWAY DOWNTOWN TAMPA VIADUCT**  
 NTS

CONCRETE REMOVAL

FINANCIAL PROJECT ID	SHEET NO.
	3-3

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.



**PROJECT CONTROLS**

**CONTEXT CLASSIFICATION**

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

**FUNCTIONAL CLASSIFICATION**

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL            ( ) LOCAL
- ( ) MINOR ARTERIAL

**HIGHWAY SYSTEM**

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

**ACCESS CLASSIFICATION**

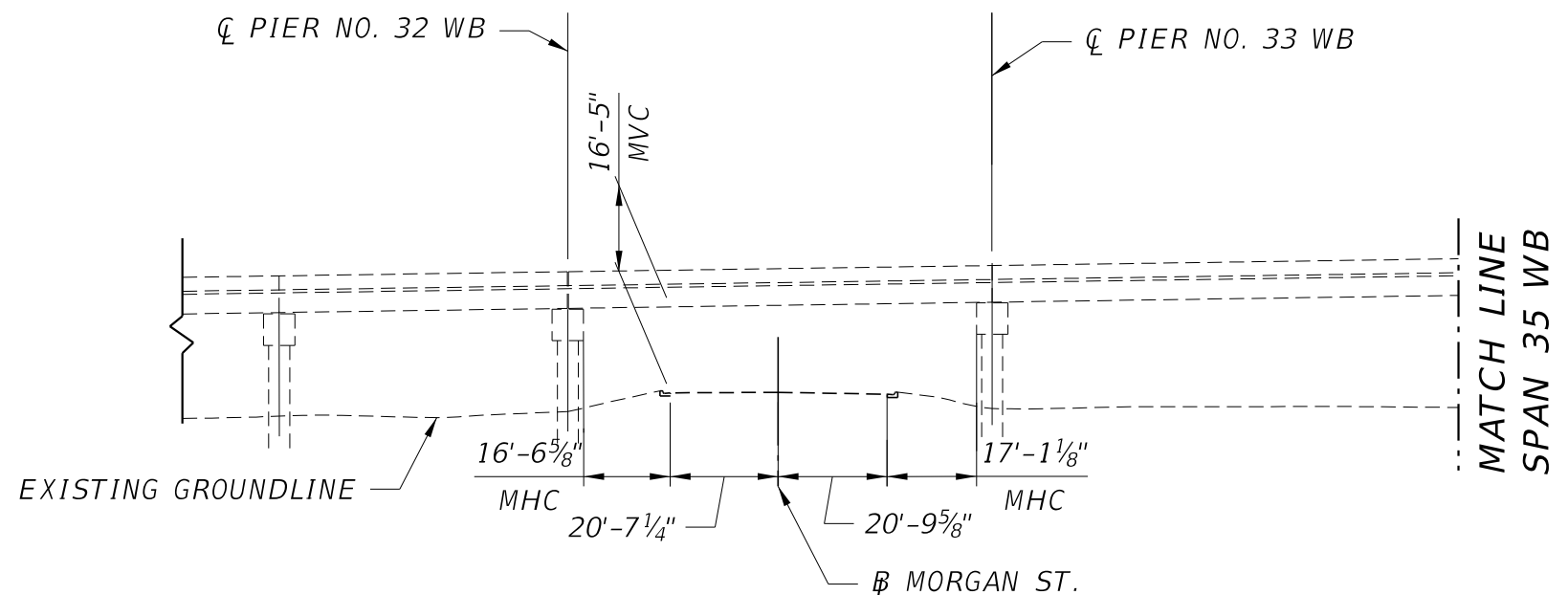
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

**CRITERIA**

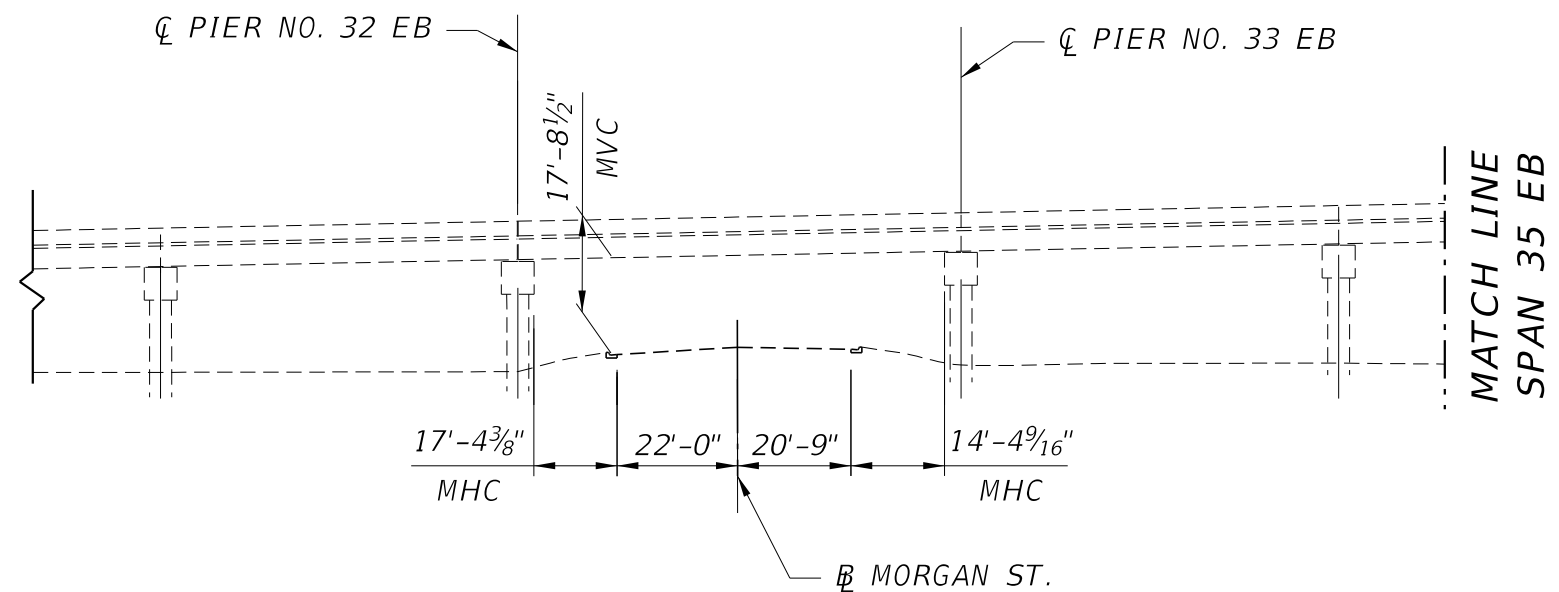
- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

**POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:**

**TYPICAL SECTION No.**



**WESTBOUND ELEVATION  
SELMON EXPRESSWAY DOWNTOWN TAMPA VIADUCT  
OVER MORGAN STREET  
NTS**



**EASTBOUND ELEVATION  
SELMON EXPRESSWAY DOWNTOWN TAMPA VIADUCT  
OVER MORGAN STREET  
NTS**

MHC = MINIMUM HORIZONTAL CLEARANCE  
MVC = MINIMUM VERTICAL CLEARANCE

FINANCIAL PROJECT ID	SHEET NO.
	3-5

**PROJECT CONTROLS**

**CONTEXT CLASSIFICATION**

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

**FUNCTIONAL CLASSIFICATION**

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL              ( ) LOCAL
- ( ) MINOR ARTERIAL

**HIGHWAY SYSTEM**

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

**ACCESS CLASSIFICATION**

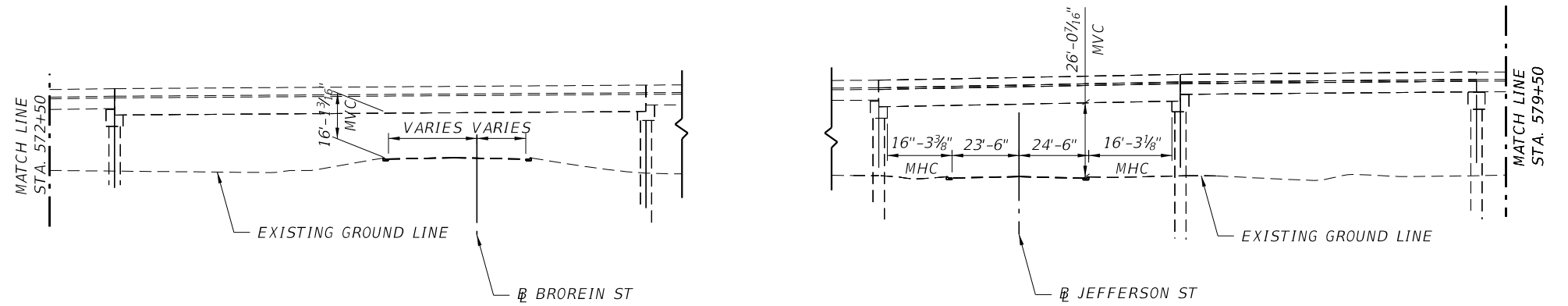
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

**CRITERIA**

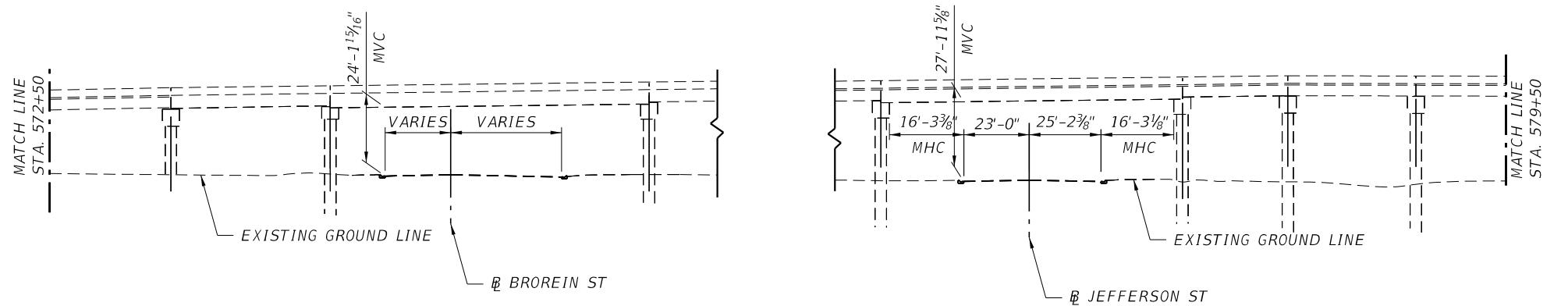
- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

**POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:**

**TYPICAL SECTION No.**



**WESTBOUND ELEVATION  
SELMON EXPRESSWAY DOWNTOWN TAMPA VIADUCT  
OVER BROREIN STREET AND JEFFERSON STREET  
NTS**



**EASTBOUND ELEVATION  
SELMON EXPRESSWAY DOWNTOWN TAMPA VIADUCT  
OVER BROREIN STREET AND JEFFERSON STREET  
NTS**

FINANCIAL PROJECT ID	SHEET NO.
	3-6

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

**CONTEXT CLASSIFICATION**

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

**FUNCTIONAL CLASSIFICATION**

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL            ( ) LOCAL
- ( ) MINOR ARTERIAL

**HIGHWAY SYSTEM**

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

**ACCESS CLASSIFICATION**

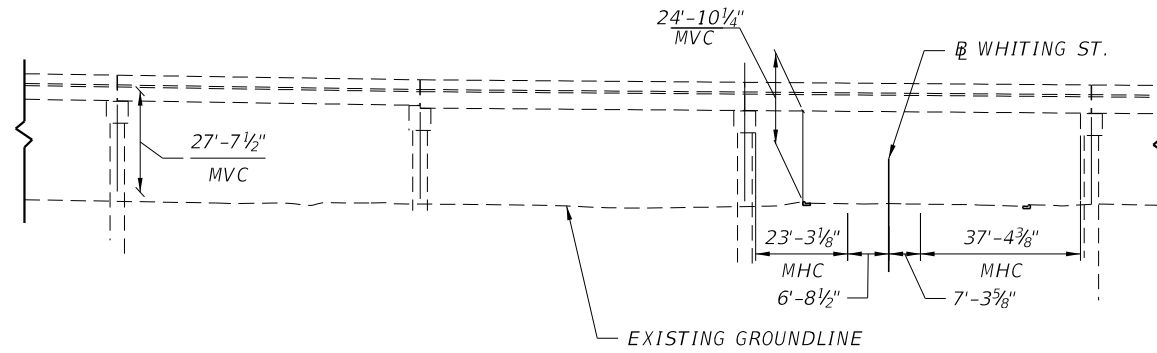
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

**CRITERIA**

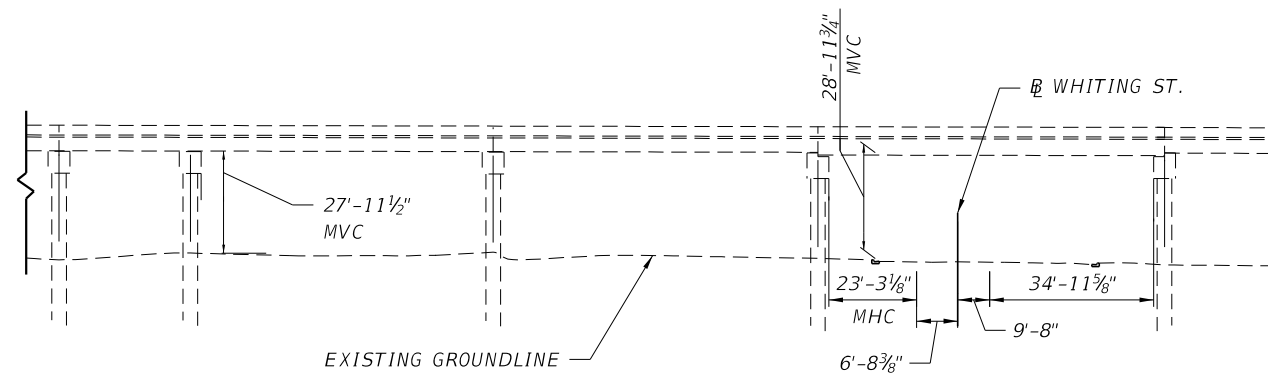
- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

**POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:**

**TYPICAL SECTION No.**



**WESTBOUND ELEVATION  
SELMON EXPRESSWAY DOWNTOWN TAMPA VIADUCT  
OVER WHITING STREET  
NTS**



**EASTBOUND ELEVATION  
SELMON EXPRESSWAY DOWNTOWN TAMPA VIADUCT  
OVER WHITING STREET  
NTS**

FINANCIAL PROJECT ID	SHEET NO.
	3-7

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

**CONTEXT CLASSIFICATION**

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

**FUNCTIONAL CLASSIFICATION**

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL            ( ) LOCAL
- ( ) MINOR ARTERIAL

**HIGHWAY SYSTEM**

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

**ACCESS CLASSIFICATION**

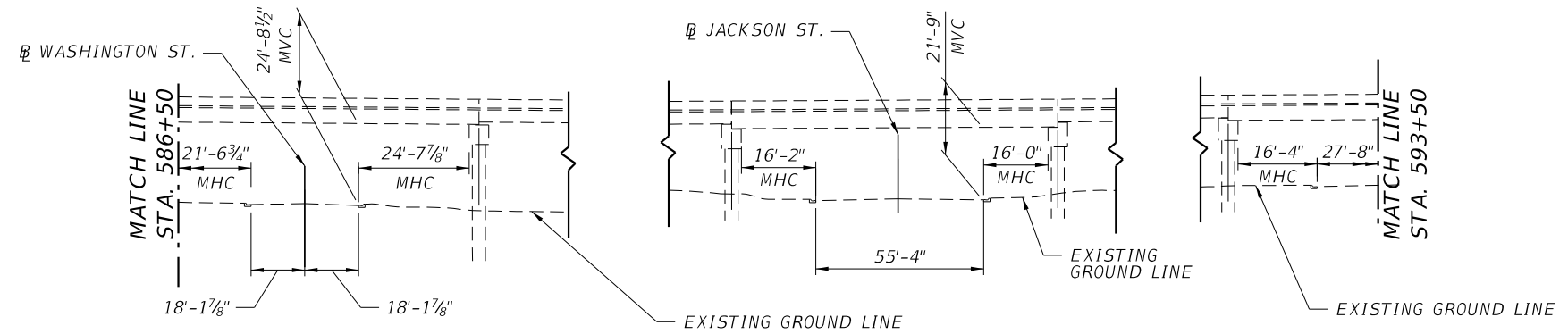
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

**CRITERIA**

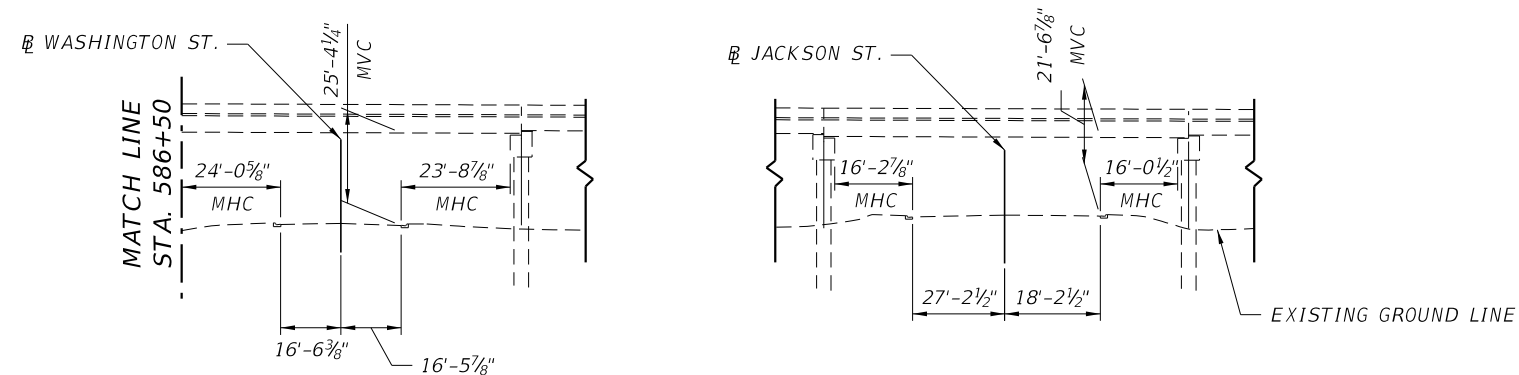
- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

**POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:**

**TYPICAL SECTION No.**



**WESTBOUND ELEVATION  
SELMON EXPRESSWAY DOWNTOWN TAMPA VIADUCT  
OVER WASHINGTON STREET AND JACKSON STREET  
NTS**



**EASTBOUND ELEVATION  
SELMON EXPRESSWAY DOWNTOWN TAMPA VIADUCT  
OVER WASHINGTON STREET AND JACKSON STREET  
NTS**

FINANCIAL PROJECT ID	SHEET NO.
	3-8

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

**CONTEXT CLASSIFICATION**

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

**FUNCTIONAL CLASSIFICATION**

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                 ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL              ( ) LOCAL
- ( ) MINOR ARTERIAL

**HIGHWAY SYSTEM**

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

**ACCESS CLASSIFICATION**

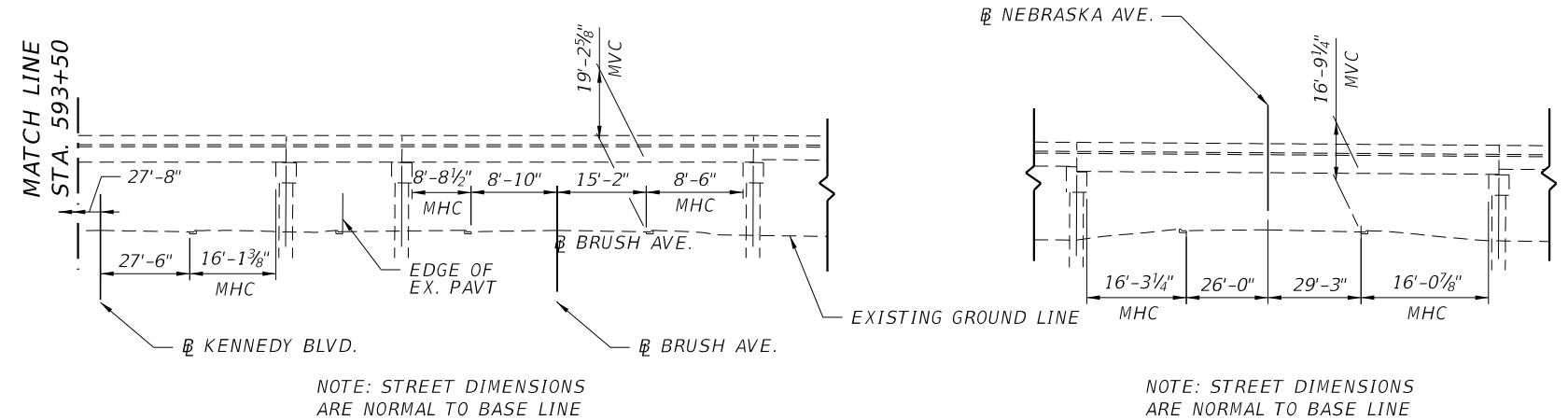
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

**CRITERIA**

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

**POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:**

**TYPICAL SECTION No.**



NOTE: STREET DIMENSIONS ARE NORMAL TO BASE LINE

NOTE: STREET DIMENSIONS ARE NORMAL TO BASE LINE

**WESTBOUND ELEVATION  
SELMON EXPRESSWAY DOWNTOWN TAMPA VIADUCT  
OVER KENNEDY BOULEVARD, BRUSH AVENUE, AND NEBRASKA AVENUE  
NTS**



NOTE: STREET DIMENSIONS ARE NORMAL TO BASE LINE

NOTE: STREET DIMENSIONS ARE NORMAL TO BASE LINE

**EASTBOUND ELEVATION  
SELMON EXPRESSWAY DOWNTOWN TAMPA VIADUCT  
OVER KENNEDY BOULEVARD, BRUSH AVENUE, AND NEBRASKA AVENUE  
NTS**

FINANCIAL PROJECT ID	SHEET NO.
	3-9

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

**CONTEXT CLASSIFICATION**

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

**FUNCTIONAL CLASSIFICATION**

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                 ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL              ( ) LOCAL
- ( ) MINOR ARTERIAL

**HIGHWAY SYSTEM**

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

**ACCESS CLASSIFICATION**

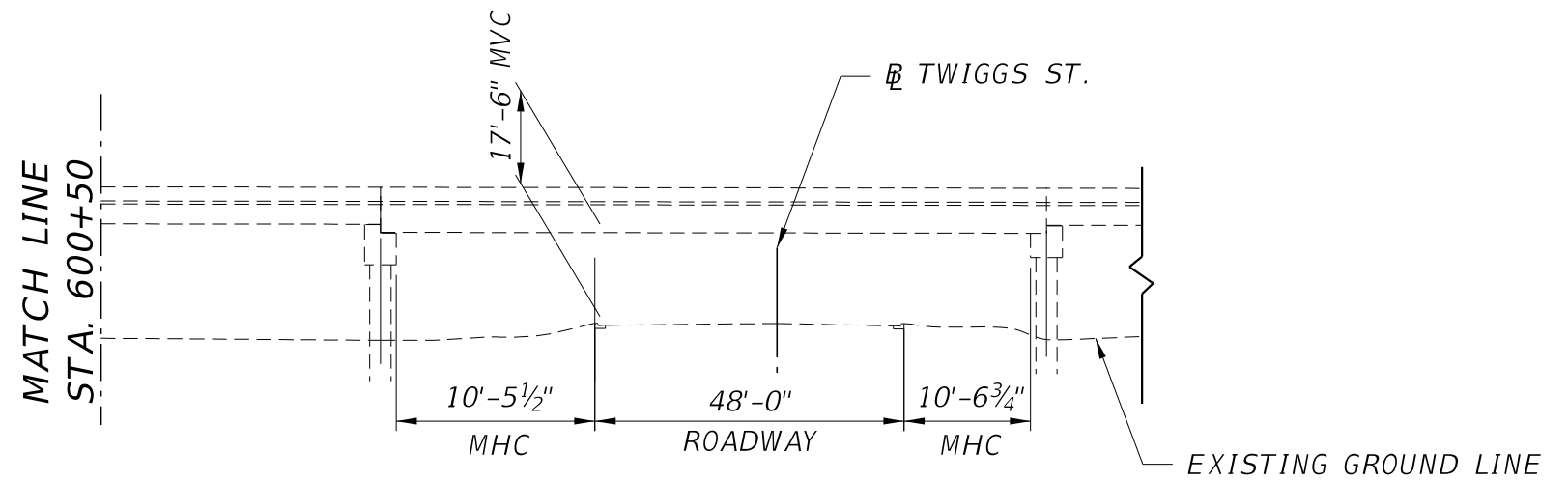
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

**CRITERIA**

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

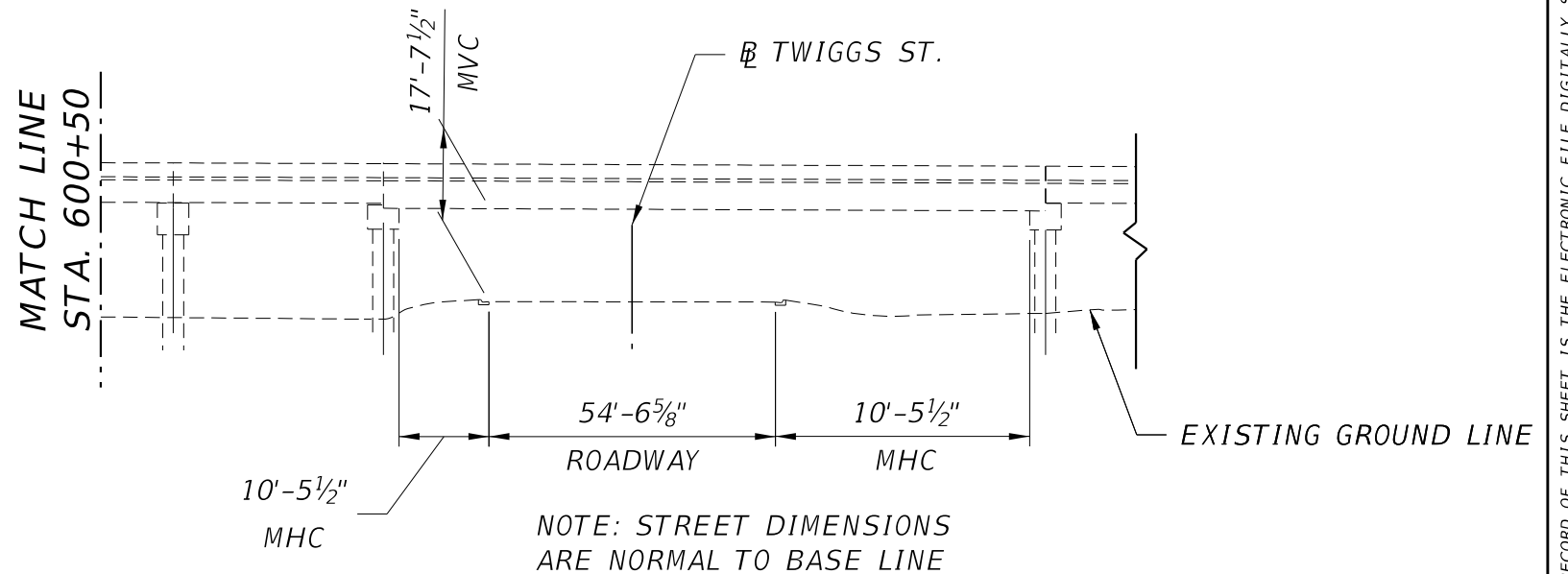
**POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:**

**TYPICAL SECTION No.**



NOTE: STREET DIMENSIONS ARE NORMAL TO BASE LINE

**WESTBOUND ELEVATION  
SELMON EXPRESSWAY DOWNTOWN TAMPA VIADUCT  
OVER TWIGGS STREET  
NTS**



NOTE: STREET DIMENSIONS ARE NORMAL TO BASE LINE

**EASTBOUND ELEVATION  
SELMON EXPRESSWAY DOWNTOWN TAMPA VIADUCT  
OVER TWIGGS STREET  
NTS**

FINANCIAL PROJECT ID	SHEET NO.
	3-10

**PROJECT CONTROLS**

**CONTEXT CLASSIFICATION**

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN            ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.        ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

**FUNCTIONAL CLASSIFICATION**

- ( ) INTERSTATE                      ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.            ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL        ( ) LOCAL
- ( ) MINOR ARTERIAL

**HIGHWAY SYSTEM**

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

**ACCESS CLASSIFICATION**

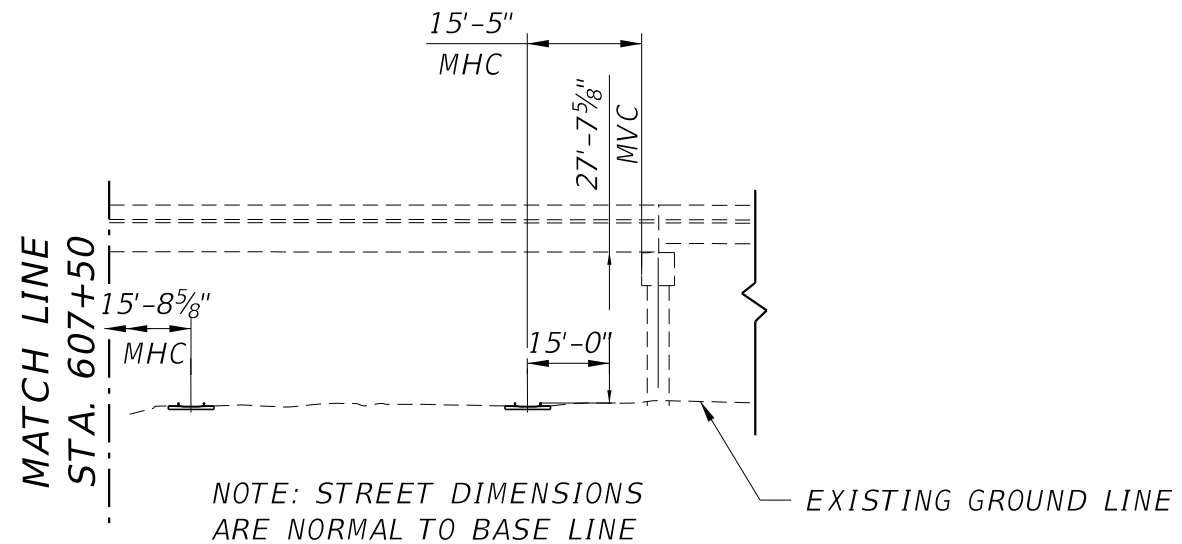
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

**CRITERIA**

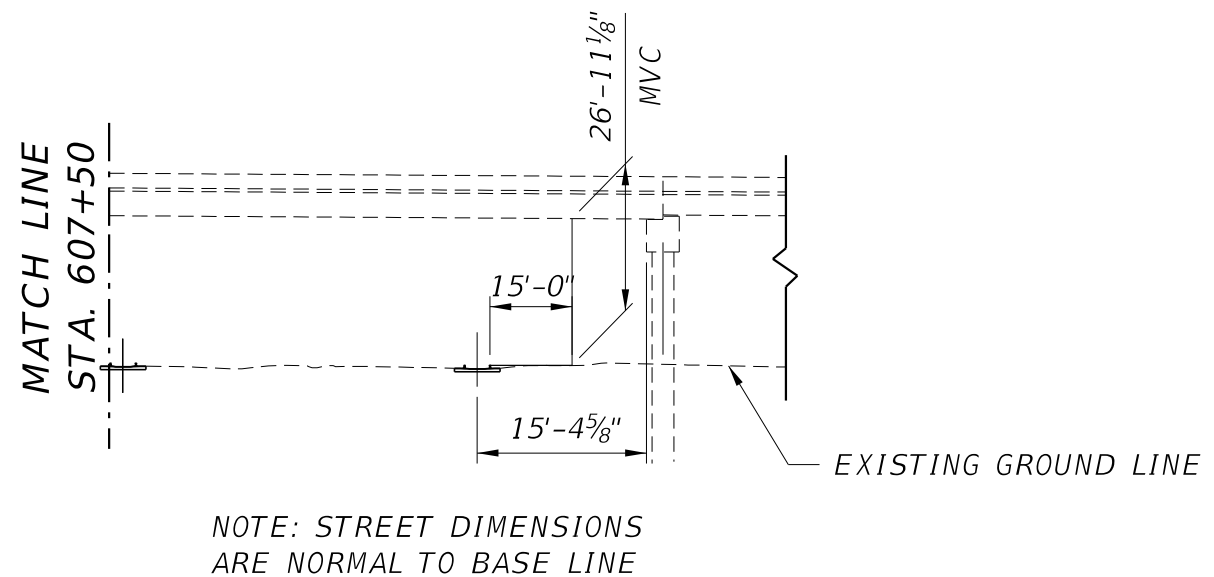
- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

**POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:**

**TYPICAL SECTION No.**



**WESTBOUND ELEVATION  
SELMON EXPRESSWAY DOWNTOWN TAMPA VIADUCT  
OVER SCL RAILROAD  
NTS**



**EASTBOUND ELEVATION  
SELMON EXPRESSWAY DOWNTOWN TAMPA VIADUCT  
OVER SCL RAILROAD  
NTS**

FINANCIAL PROJECT ID	SHEET NO.
	3-11

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

**CONTEXT CLASSIFICATION**

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

**FUNCTIONAL CLASSIFICATION**

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL            ( ) LOCAL
- ( ) MINOR ARTERIAL

**HIGHWAY SYSTEM**

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

**ACCESS CLASSIFICATION**

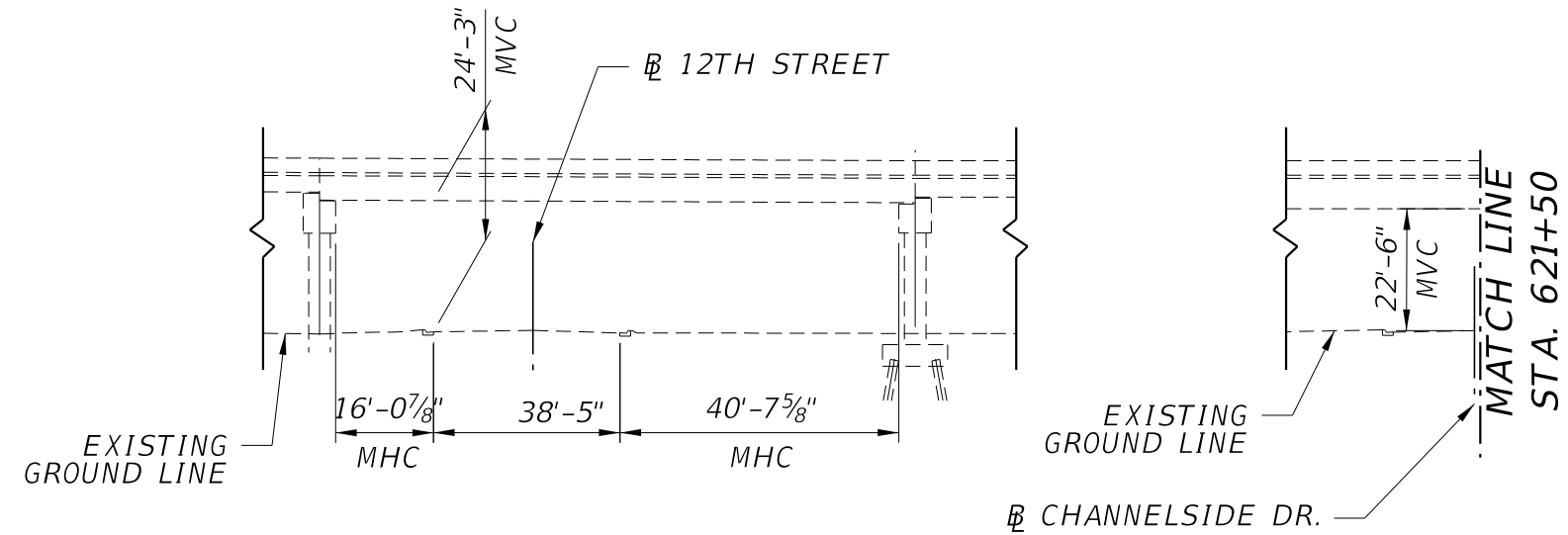
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

**CRITERIA**

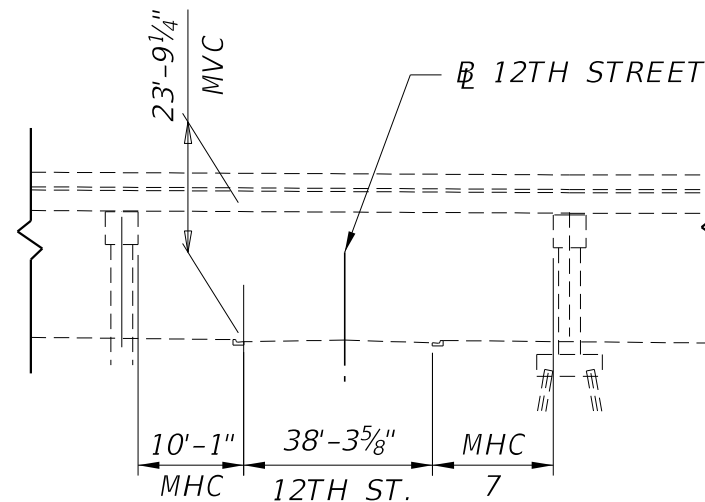
- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

**POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:**

**TYPICAL SECTION No.**



**WESTBOUND ELEVATION  
SELMON EXPRESSWAY DOWNTOWN TAMPA VIADUCT  
OVER 12TH STREET  
NTS**



**EASTBOUND ELEVATION  
SELMON EXPRESSWAY DOWNTOWN TAMPA VIADUCT  
OVER 12TH STREET  
NTS**

FINANCIAL PROJECT ID	SHEET NO.
	3-12

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

**CONTEXT CLASSIFICATION**

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

**FUNCTIONAL CLASSIFICATION**

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL            ( ) LOCAL
- ( ) MINOR ARTERIAL

**HIGHWAY SYSTEM**

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

**ACCESS CLASSIFICATION**

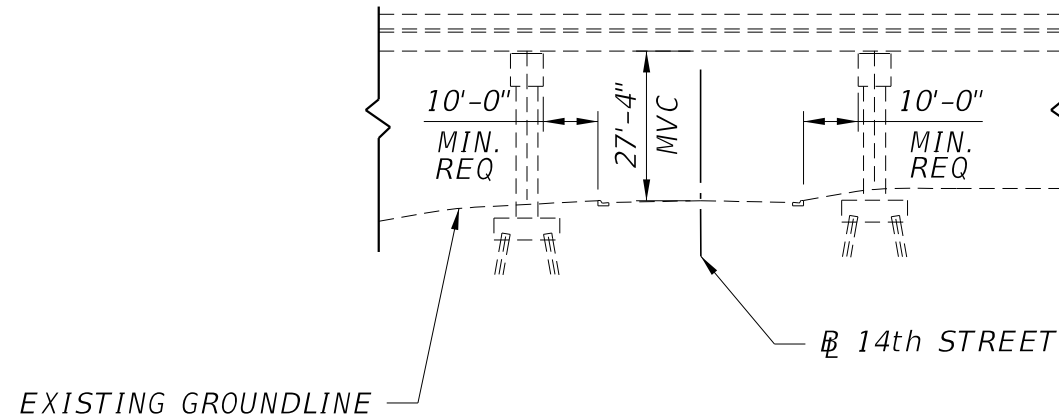
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

**CRITERIA**

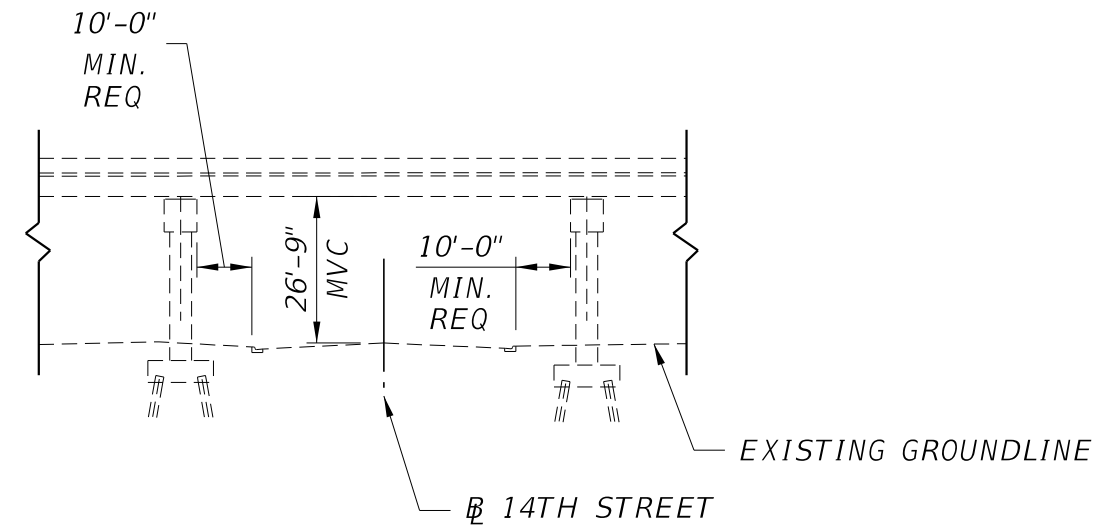
- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

**POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:**

**TYPICAL SECTION No.**



**WESTBOUND ELEVATION  
SELMON EXPRESSWAY DOWNTOWN TAMPA VIADUCT  
OVER 14TH STREET  
NTS**



**EASTBOUND ELEVATION  
SELMON EXPRESSWAY DOWNTOWN TAMPA VIADUCT  
OVER 14TH STREET  
NTS**

FINANCIAL PROJECT ID	SHEET NO.
	3-13

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

**CONTEXT CLASSIFICATION**

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.              ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

**FUNCTIONAL CLASSIFICATION**

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                 ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL              ( ) LOCAL
- ( ) MINOR ARTERIAL

**HIGHWAY SYSTEM**

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

**ACCESS CLASSIFICATION**

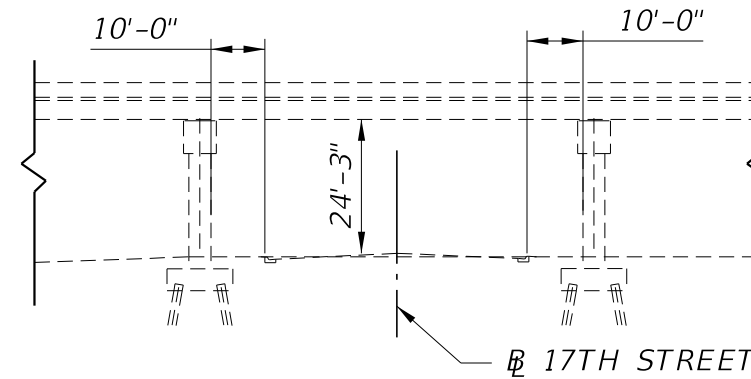
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

**CRITERIA**

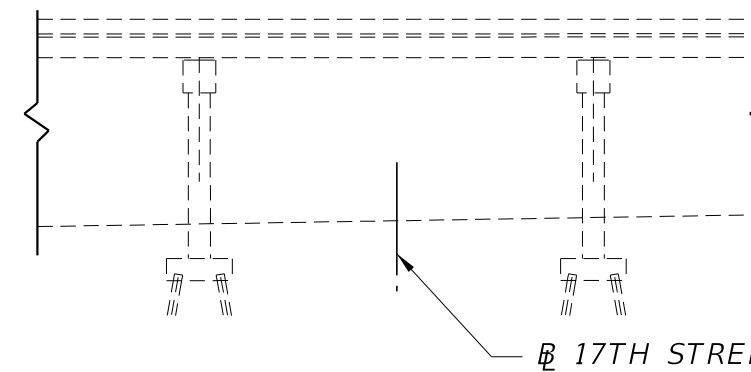
- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

**POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:**

**TYPICAL SECTION No.**



**WESTBOUND ELEVATION  
SELMON EXPRESSWAY DOWNTOWN TAMPA VIADUCT  
OVER 17TH STREET  
NTS**



**EASTBOUND ELEVATION  
SELMON EXPRESSWAY DOWNTOWN TAMPA VIADUCT  
OVER 17TH STREET  
NTS**

FINANCIAL PROJECT ID	SHEET NO.
	3-14

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL            ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

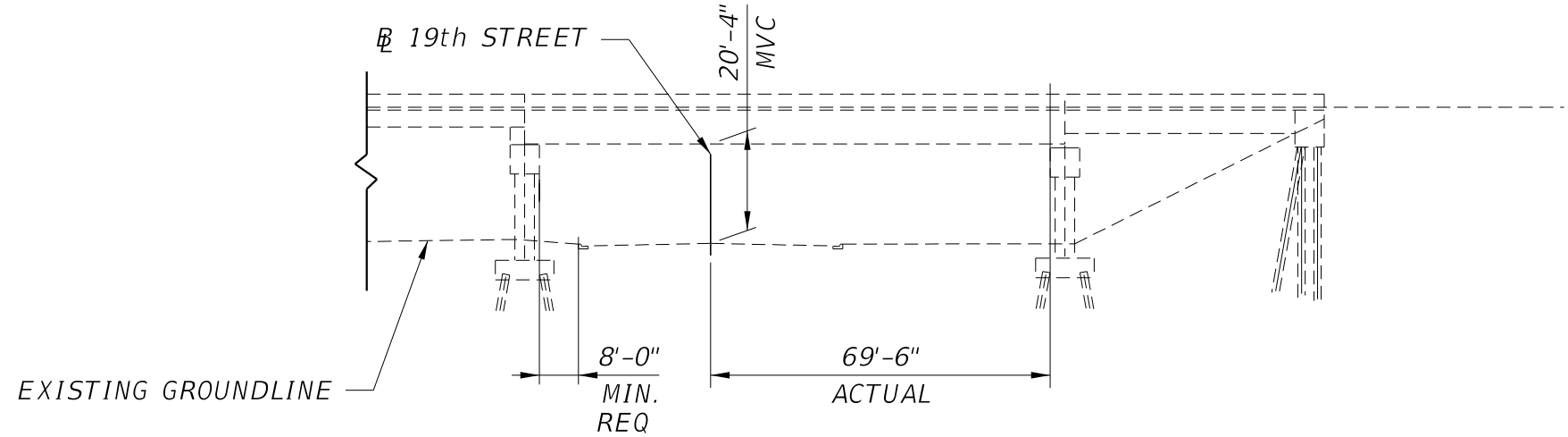
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

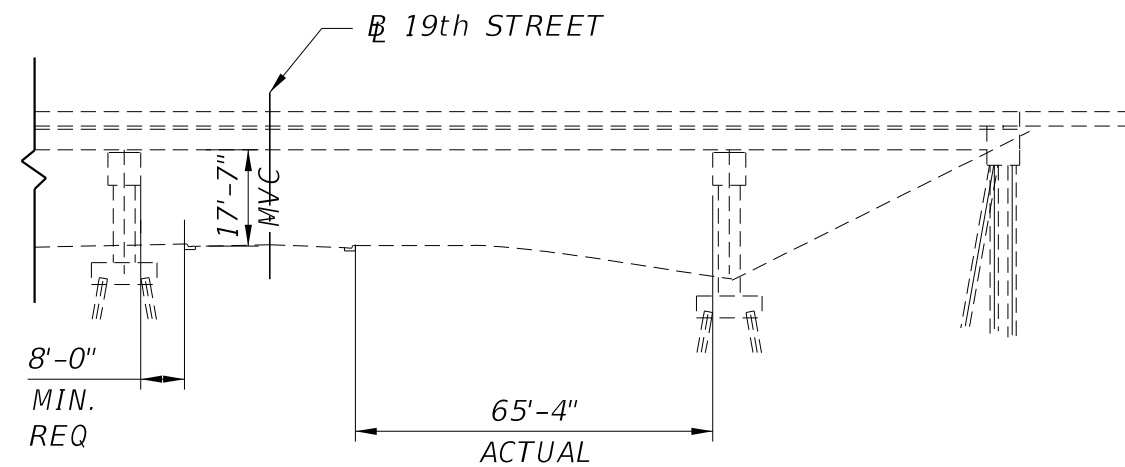
- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



**WESTBOUND ELEVATION  
SELMON EXPRESSWAY DOWNTOWN TAMPA VIADUCT  
OVER 17TH STREET  
NTS**



**EASTBOUND ELEVATION  
SELMON EXPRESSWAY DOWNTOWN TAMPA VIADUCT  
OVER 17TH STREET  
NTS**

FINANCIAL PROJECT ID	SHEET NO.
	3-15

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

# PHASE 4

REVISIONS				WSP USA Inc. 2202 North West Shore Blvd., Suite 300 Tampa, Florida 33607 (813) 520-4444 Cert. of Auth. No. 1462	TAMPA HILLSBOROUGH EXPRESSWAY AUTHORITY			SHEET NO.
DATE	DESCRIPTION	DATE	DESCRIPTION		ROAD NO.	COUNTY	FINANCIAL PROJECT ID	
						618	HILLSBOROUGH	
							4	

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL            ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL            (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN    ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES. ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE            ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.    ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

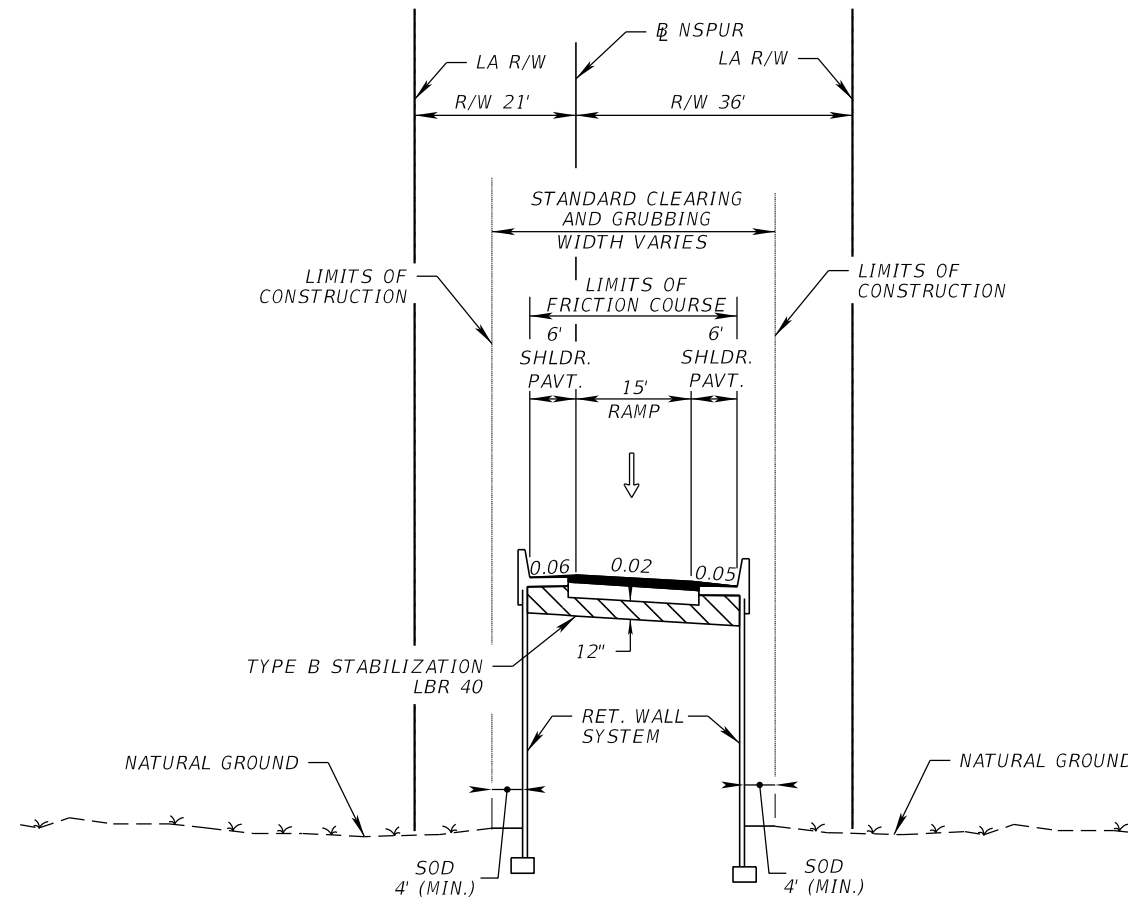
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

**TYPICAL SECTION No.**



**TYPICAL SECTION**  
**1-LANE RAMP**  
**NTS**  
 DESIGN SPEED 35  
 STA. 12+88.16 TO STA. 19+78.50 (Ⓡ NSPUR)

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

FINANCIAL PROJECT ID	SHEET NO.
	4-1

**PROJECT CONTROLS**

**CONTEXT CLASSIFICATION**

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

**FUNCTIONAL CLASSIFICATION**

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                 ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL              ( ) LOCAL
- ( ) MINOR ARTERIAL

**HIGHWAY SYSTEM**

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

**ACCESS CLASSIFICATION**

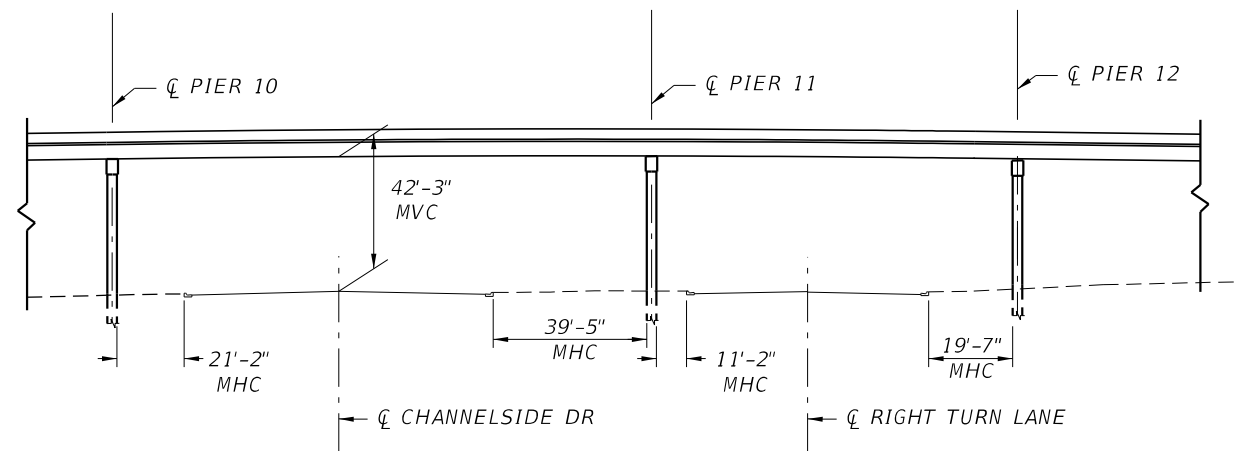
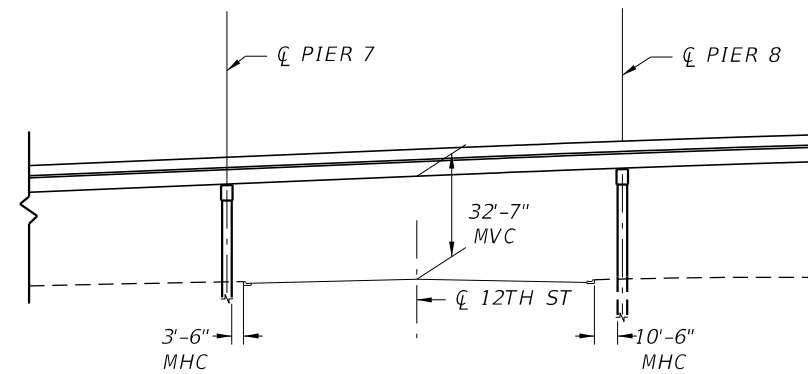
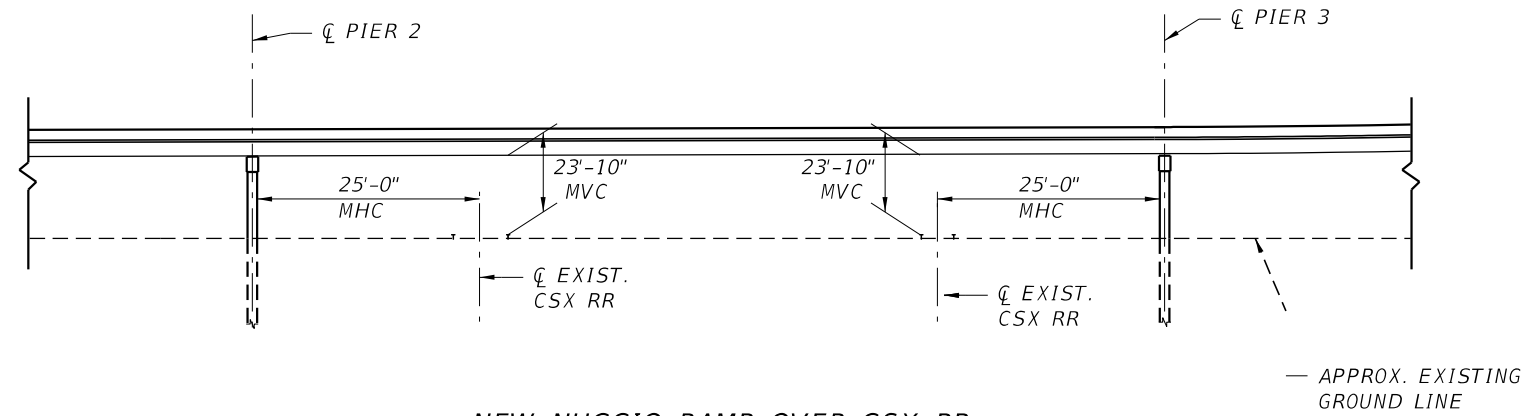
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

**CRITERIA**

- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

**POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:**

**TYPICAL SECTION No.**



MHC = MINIMUM HORIZONTAL CLEARANCE  
MVC = MINIMUM VERTICAL CLEARANCE

**ELEVATION  
NTS**

FINANCIAL PROJECT ID	SHEET NO.
	4-2

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL                      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL                        (x) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN                ( ) C5 : URBAN CENTER
- ( ) C3R : SUBURBAN RES.            ( ) C6 : URBAN CORE
- ( ) N/A : L.A. FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE                        ( ) MAJOR COLLECTOR
- (x) FREEWAY/EXPWY.                ( ) MINOR COLLECTOR
- (x) PRINCIPAL ARTERIAL            ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- (x) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- ( ) OFF-STATE HIGHWAY SYSTEM

ACCESS CLASSIFICATION

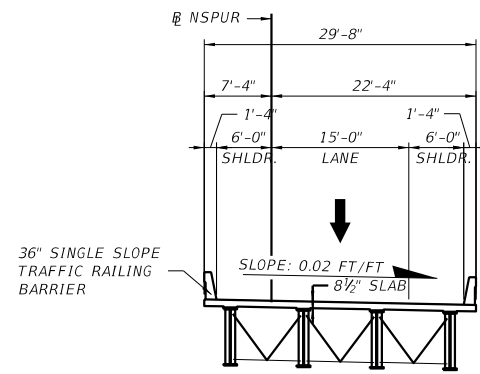
- (x) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE w/Service Roads
- ( ) 3 - RESTRICTIVE w/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE w/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE w/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE w/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

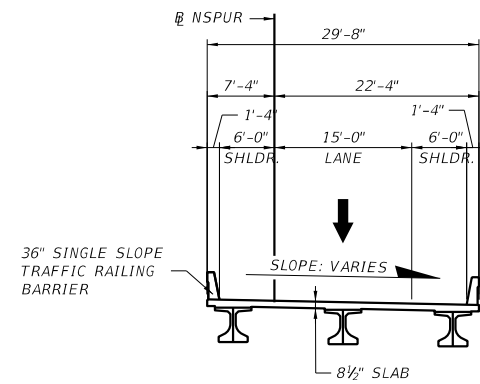
- (x) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:

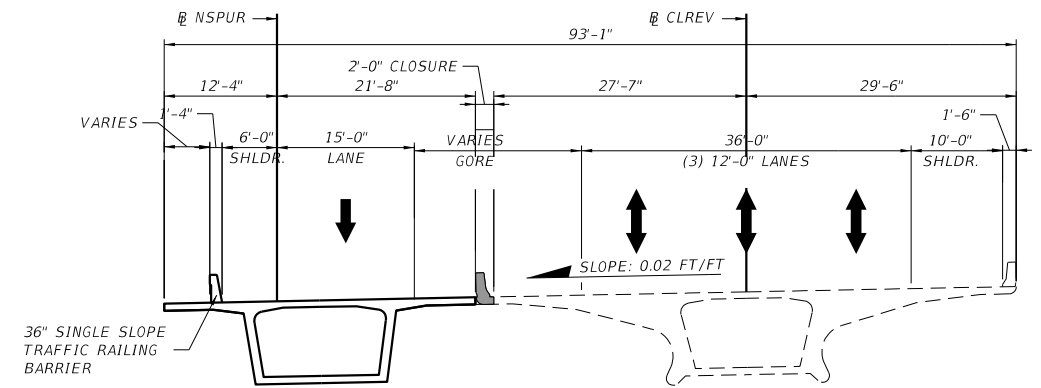
**TYPICAL SECTION No.**



PROPOSED TYPICAL SECTION SPANS 1 THROUGH 3



PROPOSED TYPICAL SECTION SPANS 4 THROUGH 9



PROPOSED TYPICAL SECTION SPANS 10 THROUGH 12

BRIDGE - NEW CONSTRUCTION  
NUCCIO SPUR RAMP  
NTS

FINANCIAL PROJECT ID	SHEET NO.
	4-3

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.